



**City of Portland, Oregon**  
**Bureau of Development Services**  
**Land Use Services**  
FROM CONCEPT TO CONSTRUCTION

Charlie Hales, Mayor  
Paul L. Scarlett, Director  
Phone: (503) 823-7300  
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[www.portlandoregon.gov/bds](http://www.portlandoregon.gov/bds)

**Date:** June 10, 2013  
**To:** Interested Person  
**From:** Chris Caruso, Land Use Services  
503-823-5747 / [Chris.Caruso@portlandoregon.gov](mailto:Chris.Caruso@portlandoregon.gov)

**NOTICE OF A TYPE II DECISION ON A PROPOSAL IN YOUR NEIGHBORHOOD**

The Bureau of Development Services has approved a proposal in your neighborhood. The reasons for the decision are included in the version located on the BDS website <http://www.portlandonline.com/bds/index.cfm?c=46429>. Click on the District Coalition then scroll to the relevant Neighborhood, and case number. If you disagree with the decision, you can appeal. Information on how to do so is included at the end of this decision.

**CASE FILE NUMBER: LU 13-133947 DZ**  
**ENTRIES, WINDOWS, SKYLIGHTS & LOADING DOCK**  
**GENERAL INFORMATION**

**Applicant:** Rich Grimes/WDC Properties  
1355 NW 13th Ave/Portland, OR 97209

**Architect:** Steve Fosler/Fosler Portland Architecture LLC  
1930 NW Lovejoy St/Portland, OR 97209

**Owner:** Overton Pearl #2 LLC  
1355 NW 13th Ave/Portland, OR 97209-3284

**Site Address:** 1315 NW OVERTON ST

**Legal Description:** BLOCK 228 LOT 1-4, COUCHS ADD  
**Tax Account No.:** R180220570, R180220570, R180220570, R180220570, R180220570  
**State ID No.:** 1N1E33AA 01400, 1N1E33AA 01400, 1N1E33AA 01400, 1N1E33AA 01400, 1N1E33AA 01400

**Quarter Section:** 2928  
**Neighborhood:** Pearl District, contact Patricia Gardner at 503-243-2628.  
**Business District:** Pearl District Business Association, Adele Nofield at 503-223-0070.  
**District Coalition:** Neighbors West/Northwest, contact Mark Sieber at 503-823-4212.  
**Plan District:** Central City - River District  
**Zoning:** EXd - Central Employment with design overlay  
**Case Type:** DZ - Design Review  
**Procedure:** Type II, an administrative decision with appeal to the Design Commission.

**Proposal:**  
The applicant seeks design review approval for exterior alterations to an existing vertical standing seam metal clad warehouse building and right-of-way loading dock in the River District of the Central City Plan District. The proposed alterations include new aluminum framed windows on both street facing facades, new wood-framed storefront systems and siding at three new recessed building entries, new decorative metal mesh along the building base, and

a new concrete loading dock in the NW 13<sup>th</sup> Avenue right-of-way with ramps, stairs, and metal railings.

Exterior alterations to existing buildings in design districts, that are not normal repair and maintenance, require design review. The proposed skylights are exempt from design review as they are replacement with like materials in existing openings.

**Relevant Approval Criteria:**

In order to be approved, this proposal must comply with the approval criteria of Title 33, Portland Zoning Code. The relevant criteria are:

- 33.825 Design Review
- Central City Fundamental Design Guidelines
- River District Design Guidelines

## ANALYSIS

**Site and Vicinity:** This site is at the end of a unique strip of buildings within the neighborhood, along the north side of NW Overton Street, which has retained its long-standing industrial character even as new development has occurred nearby. This area is likely to remain a mix of industrial buildings with newer buildings interspersed throughout as these low scale structures are well sited below and near the large Fremont Bridge and I-405 Freeway that bounds this northern section of the River District, in contrast to the glassy or brick character of much of the rest District to the east.

NW Overton Street is designated as a Traffic Access Street, City Bikeway, City Walkway, (the site is also within the Northwest Triangle Pedestrian District) and a Local Service Street for all other modes in the City’s Transportation System Plan. The site frontage along this street is currently improved with paving, curbs and sidewalks that meet City Standards. NW 13<sup>th</sup> Avenue is designated a Local Service Walkway and Local Service Bikeway in the City’s Transportation System Plan. The site frontage along this street is currently improved with paving, curbs and a raised loading dock. The site is located one block north of the Portland Streetcar which runs long NW Northrup Street and has stops at NW 14<sup>th</sup> and NW 13<sup>th</sup> Avenues.

**Zoning:** The Central Employment (EX) zone allows mixed uses and is intended for areas in the center of the City that have predominantly industrial-type development. The intent of the zone is to allow industrial and commercial uses which need a central location. Residential uses are allowed, but are not intended to predominate or set development standards for other uses in the area.

The “d” overlay promotes the conservation and enhancement of areas of the City with special historic, architectural or cultural value. New development and exterior modifications to existing development are subject to design review. This is achieved through the creation of design districts and applying the Design Overlay Zone as part of community planning projects, development of design guidelines for each district, and by requiring design review. In addition, design review ensures that certain types of infill development will be compatible with the neighborhood and enhance the area.

The Central City Plan District implements the Central City Plan and other plans applicable to the Central City area. These other plans include the Downtown Plan, the River District Plan, the University District Plan, and the Central City Transportation management Plan. The Central City plan district implements portions of these plans by adding code provisions which address special circumstances existing in the Central City area. The site is within the North Pearl Subarea and River District area of this plan district.

**Land Use History:** City records indicate no prior land use reviews for this site.

**Agency Review:** A “Notice of Proposal in Your Neighborhood” was mailed **April 29, 2013**. The following Bureaus have responded with no issues or concerns:

- Life Safety Review Section of BDS (Exhibit E-1)

The Bureau of Transportation Engineering responded with the following comment (Exhibit E-2):  
PBOT has no objections to the proposed Design Review, subject to the following condition of approval: *The applicant must receive an approved Encroachment Permit from PBOT for the requested (existing) dock along the site's NW 13<sup>th</sup> Ave frontage prior to the issuance of a Building Permit for the project.*

**Neighborhood Review:** A Notice of Proposal in Your Neighborhood was mailed on April 29, 2013. No written responses have been received from either the Neighborhood Association or notified property owners in response to the proposal.

## ZONING CODE APPROVAL CRITERIA

### Chapter 33.825 Design Review

#### Section 33.825.010 Purpose of Design Review

Design review ensures that development conserves and enhances the recognized special design values of a site or area. Design review is used to ensure the conservation, enhancement, and continued vitality of the identified scenic, architectural, and cultural values of each design district or area. Design review ensures that certain types of infill development will be compatible with the neighborhood and enhance the area. Design review is also used in certain cases to review public and private projects to ensure that they are of a high design quality.

#### Section 33.825.055 Design Review Approval Criteria

A design review application will be approved if the review body finds the applicant to have shown that the proposal complies with the design guidelines for the area.

**Findings:** The site is designated with design overlay zoning (d), therefore the proposal requires Design Review approval. Because of the site's location, the applicable design guidelines are the Central City Fundamental Design Guidelines and the River District Design Guidelines.

#### Central City Fundamental Design Guidelines

These guidelines provide the constitutional framework for all design review areas in the Central City.

The Central City Fundamental Design Guidelines and the River District Design Guidelines focus on four general categories. **(A) Portland Personality**, addresses design issues and elements that reinforce and enhance Portland's character. **(B) Pedestrian Emphasis**, addresses design issues and elements that contribute to a successful pedestrian environment. **(C) Project Design**, addresses specific building characteristics and their relationships to the public environment. **(D) Special Areas**, provides design guidelines for the four special areas of the Central City.

#### Central City Plan Design Goals

This set of goals are those developed to guide development throughout the Central City. They apply within the River District as well as to the other seven Central City policy areas. The nine goals for design review within the Central City are as follows:

1. Encourage urban design excellence in the Central City;
2. Integrate urban design and preservation of our heritage into the development process;
3. Enhance the character of the Central City's districts;
4. Promote the development of diversity and areas of special character within the Central City;
5. Establish an urban design relationship between the Central City's districts and the Central City as a whole;
6. Provide for a pleasant, rich and diverse pedestrian experience for pedestrians;
7. Provide for the humanization of the Central City through promotion of the arts;

8. Assist in creating a 24-hour Central City which is safe, humane and prosperous;
9. Ensure that new development is at a human scale and that it relates to the scale and desired character of its setting and the Central City as a whole.

### **River District Design Guidelines**

These guidelines are intended to serve as a supplement to the Central City Plan Fundamental Design Guidelines. The River District guidelines build on the basic framework, addressing design issues and opportunities which are specific to the River District. This document was adopted by City Council on February 21, 1996.

### **River District Design Goals**

The River District is planned to become a place that is remarkable within the region. The area is intended to grow rich with special and diverse qualities that are, or will become characteristic of Portland. Further, the River District will accommodate a significant portion of the region's population growth. The plan calls for the creation of a new community of dense neighborhoods, housing a resident population of over 15,000 people, and providing jobs, services, and recreation to this population and others. This area is intended to emphasize the joy of the river, connections to it, and create a sense of community. The goals frame the urban design direction for Central City and River District development.

1. Extend the river into the community to develop a functional and symbolic relationship with the Willamette River.
2. Create a community of distinct neighborhoods that accommodates a significant part of the region's residential growth.
3. Enhance the District's character and livability by fostering attractive design and activities that give comfort, convenience, safety and pleasure to all its residents and visitors.
4. Strengthen connections within River District, and to adjacent areas.

*Staff has considered all guidelines and has addressed only those guidelines considered applicable to this project.*

**A6. Reuse/Rehabilitate/Restore Buildings.** Where practical, reuse, rehabilitate, and restore buildings and/or building elements.

**A5-1-1. Reinforce the Identity of the Pearl District Neighborhood.** This guideline may be accomplished by:

- a. Recognizing the urban warehouse character of the Pearl District when altering existing buildings, and when designing new ones. Designs should provide a unified, monolithic tripartite composition (base/middle/top), with distinct cornice lines. Suburban dwelling forms should be avoided; or
- b. Celebrating and encouraging the concentration of art galleries and studios with design features that contribute to the Pearl District's "arts" ambiance. Consider features that provide connectivity and continuity such as awnings, street banners, special graphics and streetscape color coordination, which link shops, galleries, entrances, display windows and buildings. Active ground level retail that opens onto and/or uses the sidewalk can contribute to the attraction of the "arts" concentration.

**Findings for A6 and A5-1-1a:** The proposal reuses the old industrial building envelope by inserting a modern glass and wood storefront system at select locations so the space can be used by a variety of future tenants, by reconstructing the existing truck loading dock for easier public access from both streets, and by adding skylights to the roof for additional passive lighting. These alterations will allow the building to remain in use for the foreseeable future. The existing simple peaked roof form of the warehouse building will remain, allowing the structure to blend in with the surrounding low-scale warehouses that reflect the area's recent industrial past. The building will also retain its tripartite form with the addition of a metal screen foundation skin indicating the base, the retention of the vertical siding as the middle, and the top will remain a simple line of cornice sheet metal. The middle of the building is further differentiated by the repeating square windows and larger areas of wood-framed storefront bays. A new loading dock,

that is currently being reviewed for a PBOT Encroachment Permit as required in Condition of Approval B, along NW 13<sup>th</sup> Avenue and new concrete entry stairs near the southeast corner highlight the solid building base and create a clear line of demarcation between the ground plane and the upper stories. *With Condition of Approval B, these guidelines are therefore met.*

**A7. Establish and maintain a sense of urban enclosure:** Define public rights-of-way by creating and maintaining a sense of urban enclosure;

**A8. Contribute to a vibrant streetscape:** Integrate building setbacks with adjacent sidewalks to increase the space for potential public use; Develop visual and physical connections into the buildings' active interior spaces from adjacent sidewalks; Use architectural elements such as atriums, grand entries and large ground-level windows to reveal important interior spaces and activities.

**Findings for A7, A8:** The proposed renovations to the existing building will reinforce the building's edge along both street lot lines by maintaining the exterior envelope and reconstructing the loading dock, as required by the Portland Bureau of Transportation as stated in Condition of Approval B, along NW 13<sup>th</sup> Avenue, which helps define the public rights-of-way. The wood and glazed entries and the punched window openings along both facades provide new visual and physical connections from the adjacent sidewalk into the building's active spaces. Entrances near the main southeast corner are recessed to create stopping and viewing spaces, as well as to emphasize important entrances into the building. The interior open floor plan and potential for multiple retail entries allows for a wide range of retail opportunities which will help ensure new activity and interest in this emerging area. *With Condition of Approval B, these guidelines are therefore met.*

**B1. Reinforce and Enhance the Pedestrian System.** Maintain a convenient access route for pedestrian travel where a public right-of-way exists or has existed. Develop and define the different zones of a sidewalk: building frontage zone, street furniture zone, movement zone, and the curb. Develop pedestrian access routes to supplement the public right-of-way system through superblocks or other large blocks.

**B1-1. Provide Human Scale to Buildings along Walkways.** Provide human scale and interest to buildings along sidewalks and walkways.

**B2. Protect the Pedestrian.** Protect the pedestrian environment from vehicular movement. Develop integrated identification, sign, and sidewalk-oriented night-lighting systems that offer safety, interest, and diversity to the pedestrian. Incorporate building equipment, mechanical exhaust routing systems, and/or service areas in a manner that does not detract from the pedestrian environment.

**Findings for B1, B1-1 & B2:** This project continues the River District paving standards for sidewalks, curbs and street furnishings that were established on this block during the renovation of buildings directly west of this site. The rebuilt sidewalks and loading dock will help define the public rights-of-way at the east end of this block and will connect these to the existing sidewalk and street system as well as to the Portland Streetcar stops one block away. These improvements, which develop and define the different zones of the sidewalk area, will provide pedestrian access between the River District, the Northwest District, and the Pearl District, which will further encourage pedestrian exploration of the North Pearl Subarea.

The new windows and entry bays increase the opportunity for views into and out of the building, creating a visual connection between the building and its surroundings. This enhances pedestrians' sense of security and makes for a more interesting, dynamic streetscape. The recessed entries and lighting will provide protection from the weather as well as outdoor lighting at a human scale. The wood and glazed entries and the punched window openings foster a stronger connection between the building and the streetscape while also enhancing the surrounding pedestrian environment. All mechanical equipment

will be located on the roof and will not impact the pedestrian environment. *These guidelines are therefore met.*

**C2. Promote Quality and Permanence in Development.** Use design principles and building materials that promote quality and permanence.

**C3. Respect Architectural Integrity.** Respect the original character of an existing building when modifying its exterior. Develop vertical and horizontal additions that are compatible with the existing building, to enhance the overall proposal's architectural integrity.

**C4. Complement the Context of Existing Buildings.** Complement the context of existing buildings by using and adding to the local design vocabulary.

**Findings for C2, C3 & C4:** The storefront/wood window frame system and aluminum framed punched opening windows are high quality materials that provide a sense of permanence to the building. All of the glass is clear which provides a transparency to these select building areas, in contrast with the existing solid metal sided walls. The decorative metal mesh base is attached with durable fasteners and will withstand possible impacts from ground level activities. The metal mesh system also protects the existing metal siding from dents. The rebuilt loading dock is constructed of solid concrete with metal railings, both materials that will last for many years. All of the proposed materials are seen in various forms throughout this area of the River District and are in keeping with the building's existing industrial skin in their simplicity of form and honesty of application. The simple window shapes and consistent pattern of the recessed entries complements the area by replicating shapes and features on surrounding buildings. *These guidelines are therefore met.*

**C5. Design for Coherency.** Integrate the different building and design elements including, but not limited to, construction materials, roofs, entrances, as well as window, door, sign, and lighting systems, to achieve a coherent composition.

**C7. Design Corners that Build Active Intersections.** Use design elements including, but not limited to, varying building heights, changes in façade plane, large windows, awnings, canopies, marquees, signs and pedestrian entrances to highlight building corners. Locate flexible sidewalk-level retail opportunities at building corners. Locate stairs, elevators, and other upper floor building access points toward the middle of the block.

**Findings for C5 & C7:** The proposed wood window/wall system and square punched windows create a clear and logical system of openings around both sides of the building. These new openings and the metal mesh foundation screen allow the original form of the building to remain clearly expressed while also introducing new elements that reflect the local aesthetic. The selected wood, aluminum, glass and metal mesh materials create a cohesive composition through their repetition around the building. The proposal activates the prominent corner of NW Overton Street and NW 13<sup>th</sup> Avenue by opening up this portion of the building with tall areas of glazing and two recessed entries. *These guidelines are therefore met.*

## DEVELOPMENT STANDARDS

Unless specifically required in the approval criteria listed above, this proposal does not have to meet the development standards in order to be approved during this review process. The plans submitted for a building or zoning permit must demonstrate that all development standards of Title 33 can be met, or have received an Adjustment or Modification via a land use review prior to the approval of a building or zoning permit.

## CONCLUSIONS

The design review process exists to promote the conservation, enhancement, and continued vitality of areas of the City with special scenic, architectural, or cultural value. The proposed façade glass wall for the existing warehouse building provides an interesting contrast between

materials in a rapidly redeveloping part of the North Pearl Subarea. In addition, the creative reuse of a remnant industrial shell allows for continued sustainability and higher use of the building. With a mix of high quality materials and enlivening of the pedestrian experience along NW 14<sup>th</sup> and Overton Streets, the proposal is a positive addition to the River District. The proposal meets the applicable design guidelines and therefore warrants approval.

## **ADMINISTRATIVE DECISION**

Approval of design review for exterior alterations to an existing vertical standing seam metal-clad warehouse building and right-of-way loading dock in the River District of the Central City Plan District that includes the following:

- New aluminum framed windows on both street facing facades;
- New wood-framed storefront systems and siding at three new recessed building entries;
- New decorative metal mesh along the building base; and
- A new concrete loading dock in the NW 13<sup>th</sup> Avenue right-of-way with ramps, stairs, and metal railings.

Approved, per the approved site plans, Exhibits C-1 through C-15 signed and dated June 6, 2013, subject to the following conditions:

- A. As part of the building permit application submittal, the following development-related condition B must be noted on each of the 4 required site plans or included as a sheet in the numbered set of plans. The sheet on which this information appears must be labeled "ZONING COMPLIANCE PAGE - Case File LU 13-133947 DZ." All requirements must be graphically represented on the site plan, landscape, or other required plan and must be labeled "REQUIRED."
- B. The applicant must receive an approved Encroachment Permit from PBOT for the requested (existing) dock along the site's NW 13<sup>th</sup> Ave frontage prior to the issuance of a Building Permit for the project.

**Staff Planner: Chris Caruso**

**Decision rendered by:**  **on June 6, 2013**  
By authority of the Director of the Bureau of Development Services

**Decision mailed: June 10, 2013**

**About this Decision.** This land use decision is **not a permit** for development. Permits may be required prior to any work. Contact the Development Services Center at 503-823-7310 for information about permits.

**Procedural Information.** The application for this land use review was submitted on March 29, 2013, and was determined to be complete on April 24, 2013.

*Zoning Code Section 33.700.080* states that Land Use Review applications are reviewed under the regulations in effect at the time the application was submitted, provided that the application is complete at the time of submittal, or complete within 180 days. Therefore this application was reviewed against the Zoning Code in effect on March 29, 2013.

ORS 227.178 states the City must issue a final decision on Land Use Review applications within 120-days of the application being deemed complete. The 120-day review period may be waived or extended at the request of the applicant. In this case, the applicant did not waive or extend the 120-day review period. Unless further extended by the applicant, **the 120 days will expire on: August 23, 2013.**

**Some of the information contained in this report was provided by the applicant.**

As required by Section 33.800.060 of the Portland Zoning Code, the burden of proof is on the applicant to show that the approval criteria are met. The Bureau of Development Services has independently reviewed the information submitted by the applicant and has included this information only where the Bureau of Development Services has determined the information satisfactorily demonstrates compliance with the applicable approval criteria. This report is the decision of the Bureau of Development Services with input from other City and public agencies.

**Conditions of Approval.** If approved, this project may be subject to a number of specific conditions, listed above. Compliance with the applicable conditions of approval must be documented in all related permit applications. Plans and drawings submitted during the permitting process must illustrate how applicable conditions of approval are met. Any project elements that are specifically required by conditions of approval must be shown on the plans, and labeled as such.

These conditions of approval run with the land, unless modified by future land use reviews. As used in the conditions, the term “applicant” includes the applicant for this land use review, any person undertaking development pursuant to this land use review, the proprietor of the use or development approved by this land use review, and the current owner and future owners of the property subject to this land use review.

**Appealing this decision.** This decision may be appealed to the Design Commission, which will hold a public hearing. Appeals must be filed **by 4:30 PM on June 24, 2013** at 1900 SW Fourth Ave. Appeals can be filed Tuesday through Friday on the first floor of the Development Services Center until 3 p.m. After 3 p.m. and Mondays, appeals must be submitted to the receptionist at the front desk on the fifth floor. **An appeal fee of \$250 will be charged.** The appeal fee will be refunded if the appellant prevails. There is no fee for ONI recognized organizations appealing a land use decision for property within the organization’s boundaries. The vote to appeal must be in accordance with the organization’s bylaws. Assistance in filing the appeal and information on fee waivers is available from BDS in the Development Services Center. Please see the appeal form for additional information.

The file and all evidence on this case are available for your review by appointment only. Please call the Request Line at our office, 1900 SW Fourth Avenue, Suite 5000, phone 503-823-7617, to schedule an appointment. I can provide some information over the phone. Copies of all information in the file can be obtained for a fee equal to the cost of services. Additional information about the City of Portland, city bureaus, and a digital copy of the Portland Zoning Code is available on the internet at [www.portlandonline.com](http://www.portlandonline.com).

**Attending the hearing.** If this decision is appealed, a hearing will be scheduled, and you will be notified of the date and time of the hearing. The decision of the Design Commission is final; any further appeal must be made to the Oregon Land Use Board of Appeals (LUBA) within 21 days of the date of mailing the decision, pursuant to ORS 197.620 and 197.830. Contact LUBA at 550 Capitol St. NE, Suite 235, Salem, Oregon 97301, or phone 1-503-373-1265 for further information.

Failure to raise an issue by the close of the record at or following the final hearing on this case, in person or by letter, may preclude an appeal to the Land Use Board of Appeals (LUBA) on that issue. Also, if you do not raise an issue with enough specificity to give the Design Commission an opportunity to respond to it, that also may preclude an appeal to LUBA on that issue.

**Recording the final decision.**

If this Land Use Review is approved the final decision must be recorded with the Multnomah County Recorder. A few days prior to the last day to appeal, the City will mail instructions to the applicant for recording the documents associated with their final land use decision.

- *Unless appealed,* The final decision may be recorded on or after **June 25, 2013.**
- A building or zoning permit will be issued only after the final decision is recorded.

The applicant, builder, or a representative may record the final decision as follows:

- By Mail: Send the two recording sheets (sent in separate mailing) and the final Land Use Review decision with a check made payable to the Multnomah County Recorder to: Multnomah County Recorder, P.O. Box 5007, Portland OR 97208. The recording fee is identified on the recording sheet. Please include a self-addressed, stamped envelope.
- In Person: Bring the two recording sheets (sent in separate mailing) and the final Land Use Review decision with a check made payable to the Multnomah County Recorder to the County Recorder's office located at 501 SE Hawthorne Boulevard, #158, Portland OR 97214. The recording fee is identified on the recording sheet.

For further information on recording, please call the County Recorder at 503-988-3034  
 For further information on your recording documents please call the Bureau of Development Services Land Use Services Division at 503-823-0625.

**Expiration of this approval.** An approval expires three years from the date the final decision is rendered unless a building permit has been issued, or the approved activity has begun.

Where a site has received approval for multiple developments, and a building permit is not issued for all of the approved development within three years of the date of the final decision, a new land use review will be required before a permit will be issued for the remaining development, subject to the Zoning Code in effect at that time.

**Applying for your permits.** A building permit, occupancy permit, or development permit may be required before carrying out an approved project. At the time they apply for a permit, permittees must demonstrate compliance with:

- All conditions imposed herein;
- All applicable development standards, unless specifically exempted as part of this land use review;
- All requirements of the building code; and
- All provisions of the Municipal Code of the City of Portland, and all other applicable ordinances, provisions and regulations of the City.

### **EXHIBITS**

NOT ATTACHED UNLESS INDICATED

- A. Applicant's Statement
- B. Zoning Map (attached)
- C. Plans/Drawings:
  - 1. Site Plan (attached)
  - 2. Title and Project Info
  - 3. As-Built Floor Plan
  - 4. Proposed Floor Plan (attached)
  - 5. As-Built Elevations
  - 6. Proposed Elevations (attached)
  - 7. Rendered Elevations
  - 8. Elevations
  - 9. Details
  - 10. Glazing Details
  - 11. Window Details
  - 12. Door Details
  - 13. Window Detail
  - 14. Rail Detail
  - 15. Details
- D. Notification information:
  - 1. Mailing list
  - 2. Mailed notice
- E. Agency Responses:
  - 1. Life Safety Review Section of BDS

2. Bureau of Transportation Engineering and Development Review

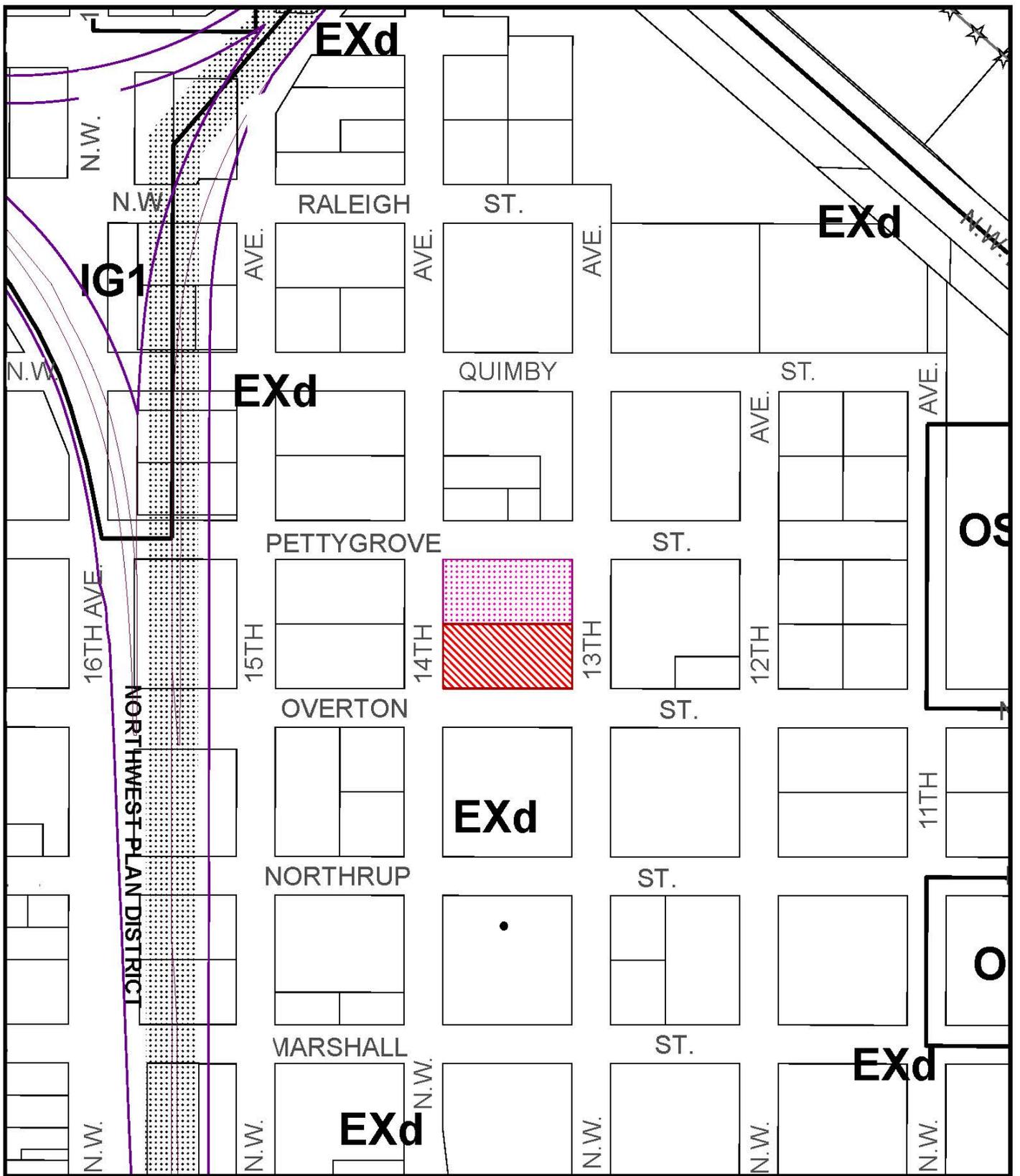
F. Correspondence: none received

G. Other:

1. Original LU Application

2. Incomplete Letter

**The Bureau of Development Services is committed to providing equal access to information and hearings. Please notify us no less than five business days prior to the event if you need special accommodations. Call 503-823-7300 (TTY 503-823-6868).**



# ZONING

-  Site
-  Also Owned



File No. LU 13-133947 DZ  
 1/4 Section 2928  
 Scale 1 inch = 200 feet  
 State\_Id 1N4050000308  
 Exhibit B (Apr 04, 2013)



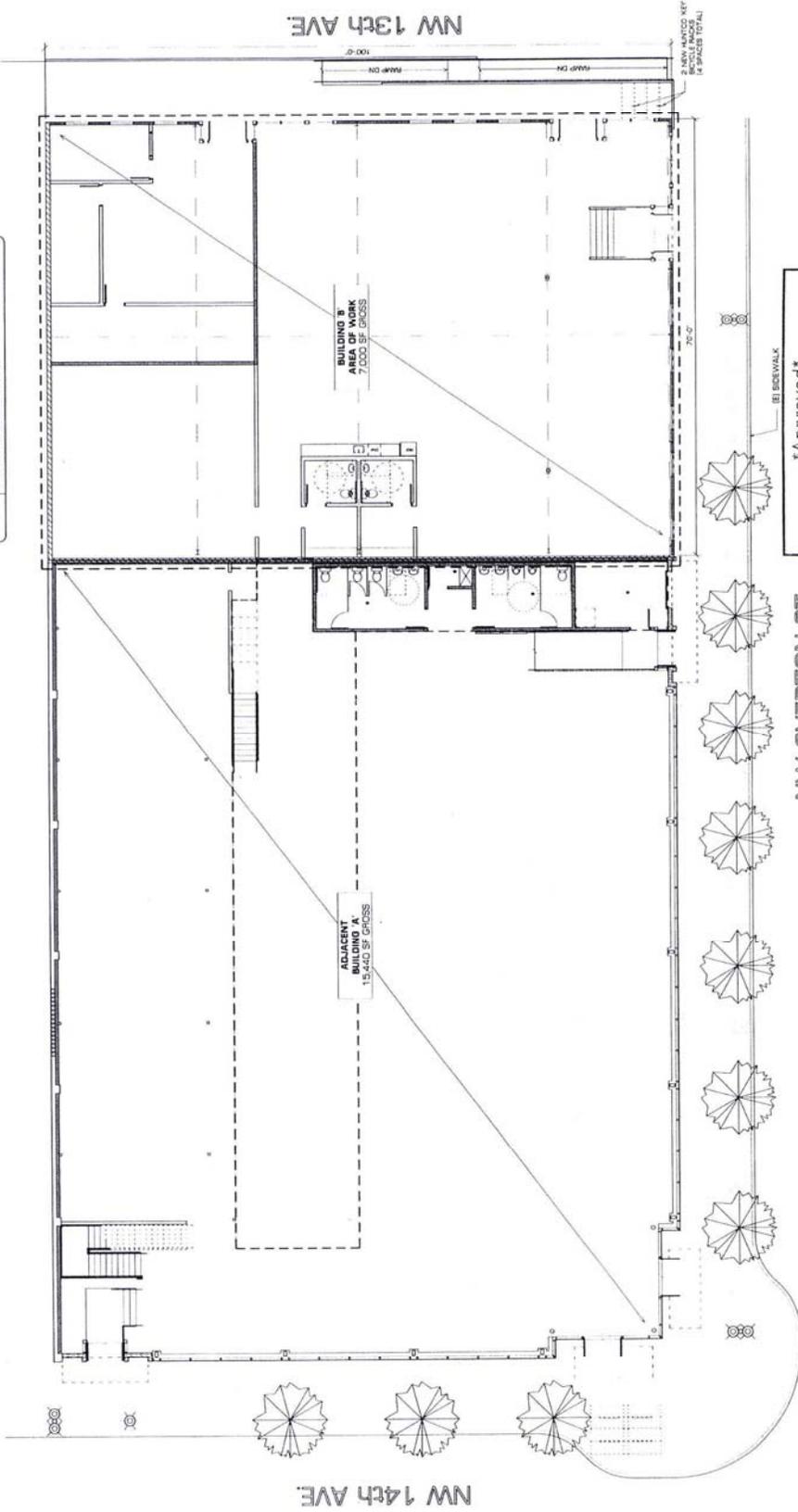
**FOSTER**  
Architecture  
Portland

1307 NW Overton Street  
**STEEL BUILDING**

T: 503.227.1111  
DESIGN: S  
2014 MA  
R1 17 A

DR  
SITE

NOT TO SCALE. ILLUSTRATIONS RESEMBLE THE APPEARANCE OF THE BUILDING. THIS EXISTING BUILDING SHALL BE ALLOWED AT ALL TIMES TO BE OCCUPIED BY THE NUMBER OF SPACES OCCUPIED BY THE BUILDING. ILLUSTRATIONS RESEMBLE THE APPEARANCE OF THE BUILDING. THIS EXISTING BUILDING SHALL BE ALLOWED AT ALL TIMES TO BE OCCUPIED BY THE NUMBER OF SPACES OCCUPIED BY THE BUILDING. ILLUSTRATIONS RESEMBLE THE APPEARANCE OF THE BUILDING. THIS EXISTING BUILDING SHALL BE ALLOWED AT ALL TIMES TO BE OCCUPIED BY THE NUMBER OF SPACES OCCUPIED BY THE BUILDING.



NW OVERTON ST.

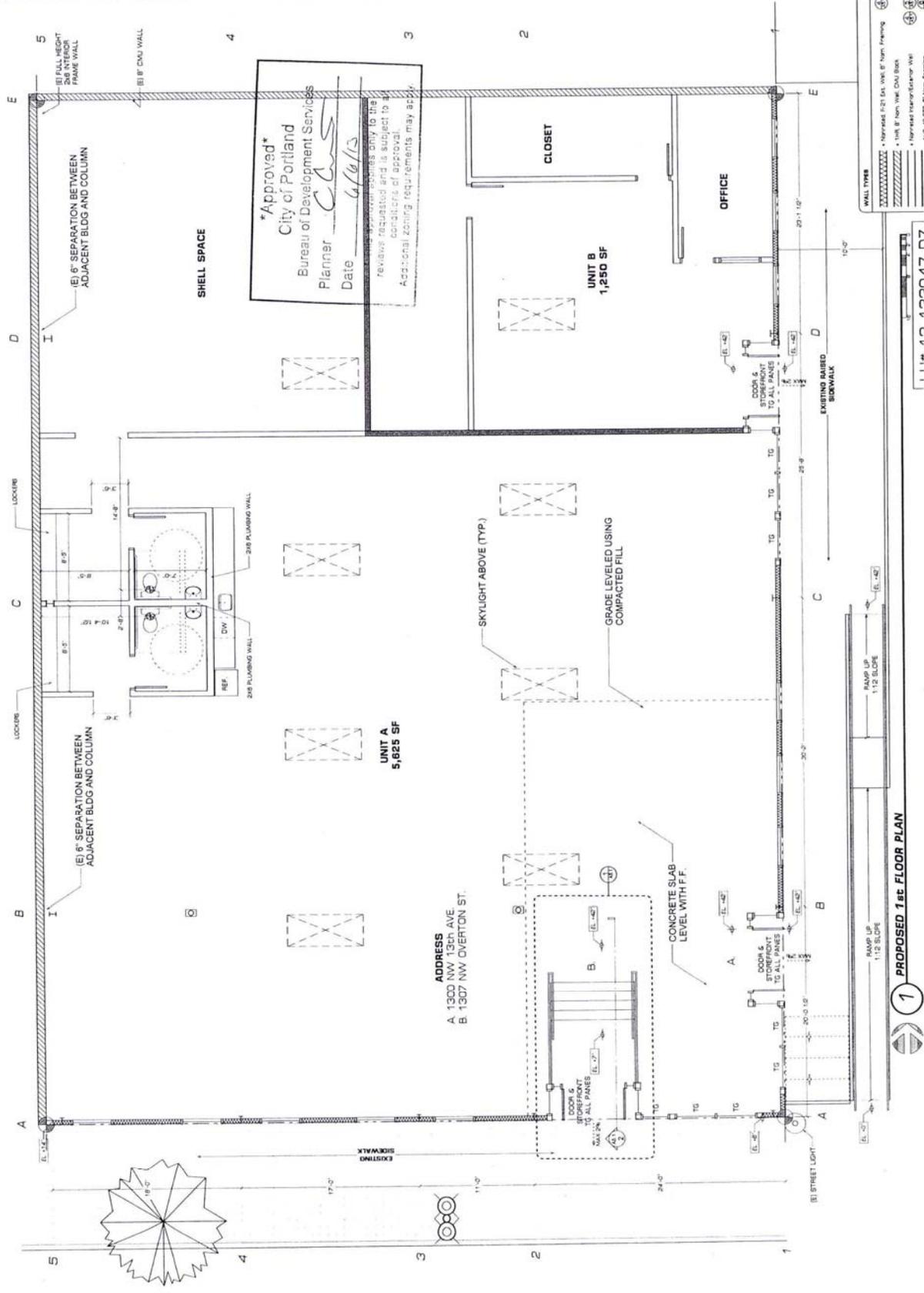
1 SITE PLAN

\*Approved\*  
City of Portland  
Bureau of Development Services  
Planner *C. S.*  
Date *6/6/13*  
\* This approval applies only to the review requested and is subject to all conditions of approval. Additional zoning requirements may apply.

**BUILDING CODE ANALYSIS**  
BUILDING TYPE: 311-B  
BUILDING FT.: 1-STORY (E)  
DETECTION: YES  
ALARM: YES  
EXISTING OCCUPANCY: F-1  
PROPOSED OCCUPANCY: B  
**ZONING CODE ANALYSIS:**  
USE CATEGORY: OFFICE  
CAR PARKING & LOADING: MINIMUM PARKING NOT REQ DUE TO ZONING

LU# 13-133947 DZ

EXH. C-1



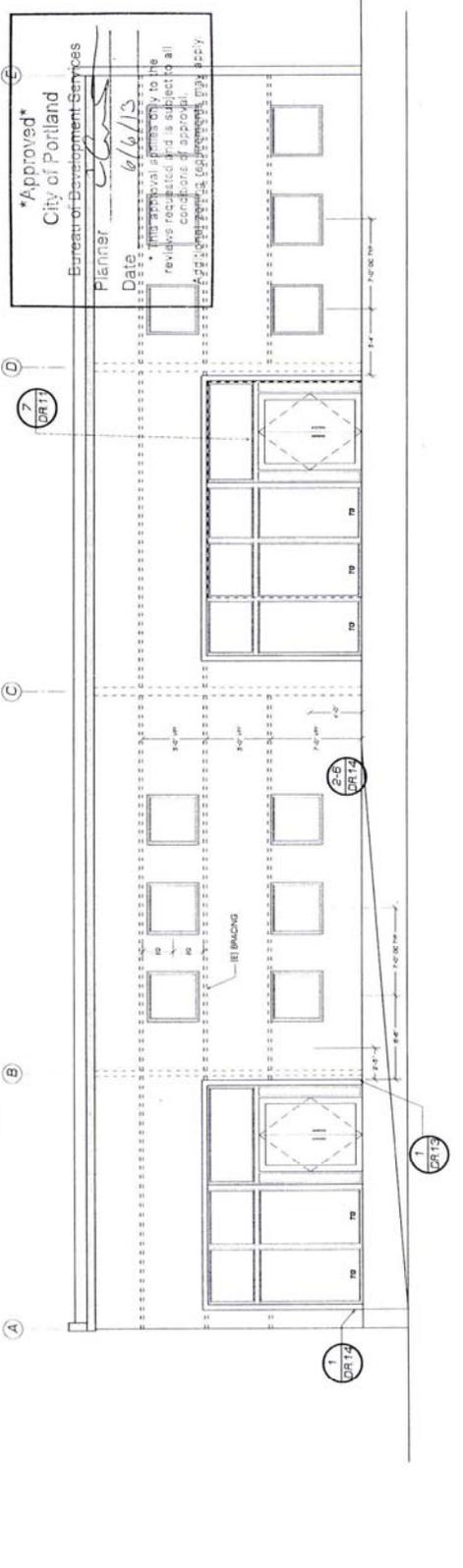
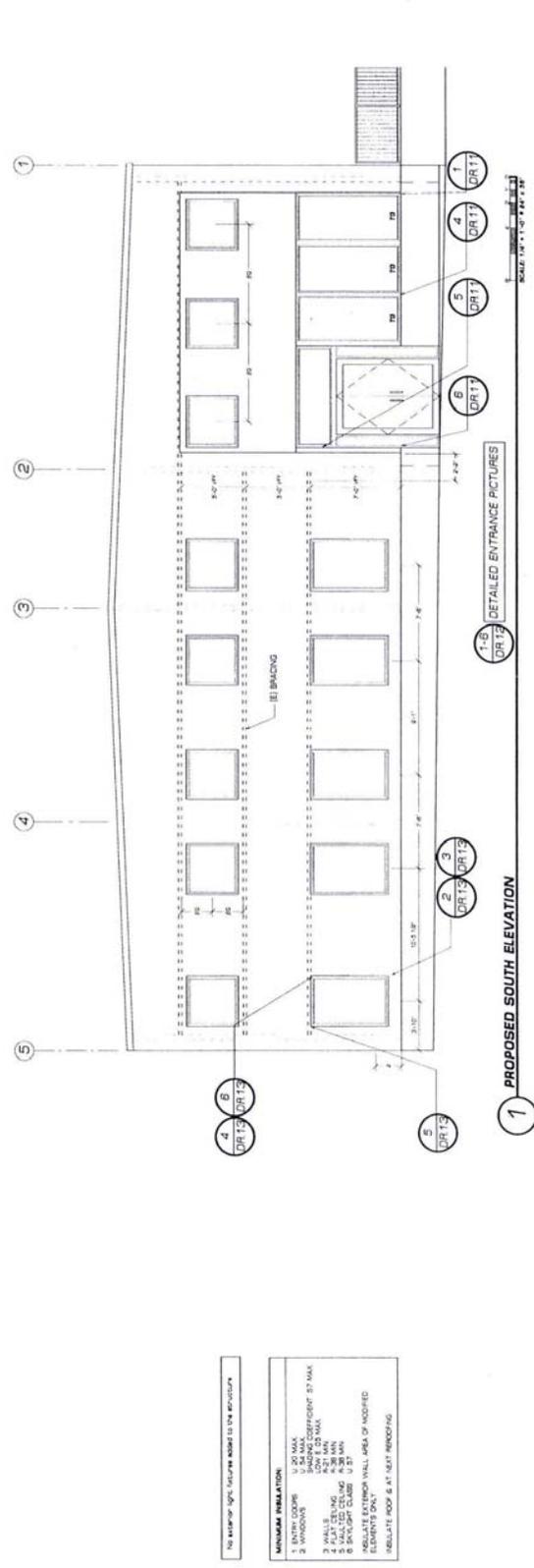
LU 13-133947 DZ EXH.C-4



1307 NW Overton Street  
STEEL BUILDING

DATE	28 M
DESIGNER	T
PROJECT	M 17

DR  
PROF. ELEV.



\*Approved\*  
City of Portland  
Bureau of Development Services  
Planner *[Signature]*  
Date 6/6/13

This approval is pending only to the reviewer's recollection and is subject to all conditions of approval.

No exterior light fixtures shown in this elevation.

MINIMUM INSULATION	
1. ENTRY DOORS	U-20 MAX
2. WINDOWS	SHADING COEFFICIENT 0.7 MAX
3. WALLS	R-13 MIN
4. FLOORS	R-20 MIN
5. CEILING	R-13 MIN
6. EXTERIOR WALLS	R-13 MIN
7. ROOF	R-30 MIN

INSULATION SHALL BE CONTINUOUS THROUGH ALL JOINTS AND PENETRATIONS.  
ELEMENTS ONLY.  
INSULATE ROOF & AT NEXT PENETRATIONS.

2 PROPOSED EAST ELEVATION  
NOTE: EXISTING HANDRAILS & GUARDRAILS NOT SHOWN  
(E) HANDRAILS \* RAMP EDGE

LU# 13-133947 DZ

EXT. C-6