



City of Portland
Bureau of Development Services
Land Use Services Division

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Portland, Oregon 97201
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Date: July 28, 2008
To: Interested Person
From: Staci Monroe, Land Use Services
503-823-7870 / Staci.Monroe@ci.portland.or.us

NOTICE OF A TYPE II DECISION ON A PROPOSAL IN YOUR NEIGHBORHOOD

The Bureau of Development Services has approved a proposal in your neighborhood. The reasons for the decision are included in this notice. If you disagree with the decision, you can appeal it and request a public hearing. Information on how to appeal this decision is listed at the end of this notice.

CASE FILE NUMBER: LU 08-138580 DZ – DESIGN REVIEW FOR A STOREFRONT REMODEL

GENERAL INFORMATION

Applicant: Western Building Company Portland LLC (owner)
621 SW Morrison Street, Suite 800
Portland, OR 97205-3825

Chris Kopca (owner)
Downtown Development Group
920 SW 6th Ave, Suite 223
Portland OR 97204

Representative: Aron Faegre (architect), 503-222-2546
Aron Faegre & Associates
520 SW Yamhill St, Roofgarden #1
Portland, OR 97204

Site Address: 709 SW SALMON STREET

Legal Description: BLOCK 209 LOT 3&4, PORTLAND
Tax Account No.: R667722480
State ID No.: 1S1E03BB 03900
Quarter Section: 3129
Neighborhood: Downtown Cmty Assoc., contact Jennifer Geske at 503-750-9843.
Business District: Downtown Retail Council, contact Portland Business Alliance at 503-224-8684.
District Coalition: Neighbors West/Northwest, contact Mark Sieber at 503-823-4212.
Plan District: Central City - Downtown
Zoning: CXd – Central Commercial zone with a Design overlay
Case Type: DZ – Design Review
Procedure: Type II, an administrative decision with appeal to the Design Commission.

Proposal:
The applicant seeks Design Review for a storefront remodel along the ground floor of an existing building. The current storefront consists of both the original wood storefront on the

western portion of the Salmon Street façade and a later addition of stucco and aluminum windows from the 1980's on the remainder of the street-facing façade. The remodel would provide for a unified storefront system of black aluminum framing with storefront glass, upper transoms and ½" thick metal faced composite infill panel along the base. Four metal louvered vents are proposed for mechanical ventilation, two within the transoms on each elevation and two within the alcove ceilings at each end of the storefront. The remodel also includes recessed can lights within each recessed entryway and rebuilding masonry columns out of brick and cast stone along both the Salmon and Broadway elevations. Other exterior improvements that do not require review include repainting the building and repairing and replacing rotted wood windows on the upper floor. The existing building signs will be removed for the remodel and reviewed under a separate Design Review application to be submitted in the near future.

Exterior alterations to existing development with a Design "d" overlay and within the Central City Plan District require Design Review.

Relevant Approval Criteria:

In order to be approved, this proposal must comply with the approval criteria of Title 33, Portland Zoning Code. The relevant approval criteria are:

- Central City Fundamental Design Guidelines

ANALYSIS

Site and Vicinity: The 10,000 SF, quarter block site is at the southwest corner of Broadway and Salmon Streets. The property is fully developed with a 18, 744 SF two-story commercial building consisting of office on the upper level and retail on the ground floor. The brick building was constructed in 1924 has gone through several remodel including the removal of most of the original wood storefront, with the exception of the transom windows along Salmon elevation, and replaced with stucco solid walls and smaller aluminum windows.

The building is situated in the Downtown subdistrict of the Central City Plan District and as well as the Downtown Pedestrian District.

Zoning: The CX (Central Commercial) zone is intended to provide for commercial development within Portland's most urban and intense areas. A broad range of uses is allowed to reflect Portland's role as a commercial, cultural and governmental center. Development is intended to be very intense with high building coverage, large buildings, and buildings placed close together.

The "d" (Design) overlay promotes the conservation and enhancement of areas of the City with special historic, architectural or cultural value. New development and exterior modifications to existing development are subject to design review.

Land Use History: City records indicate that prior land use reviews include the following:

- DZ 65-83: Approval of a 1983 Design Review for signage.
- DZ 67-85: Approval of a 1985 Design Review for signage.
- DZ 50-89: Approval of a 1989 Design Review for a storefront remodel.
- DZ 069-90: Approval of a 1990 Design Review for a storefront addition on SW Salmon Street.
- LUR 91-00822 DZ: Approval of a 1991 Design Review for removal of a marquee and to recess a portion of the storefront and two doors by 14'-0".
- LUR 95-00468 DZ: Approval of a 1995 Design Review for a rooftop noise control barrier with galvanized steel framing and siding.

Agency Review: A "Notice of Proposal in Your Neighborhood" was mailed **June 30, 2008**. The following Bureaus have responded with no issues or concerns:

- Bureau of Environmental Services
- Water Bureau
- Fire Bureau
- Site Development Section of BDS
- Bureau of Parks-Forestry Division

The Plan Review Section of BDS responded with the following comment (please see Exhibit E-1 for additional details):

- Based on the information provided, there appears to be no conflicts between the proposal and applicable building codes for the purpose of obtaining an LU approval.
- A separate Building Permit is required for the work proposed and the proposal must be designed to meet all applicable building codes and ordinances. More information regarding building code requirements can be obtained by visiting the Bureau of Development Services Development Services Center -1900 SW 4th Ave, 1st floor, between the hours of 7:30 AM and 3:00 PM Monday through Friday and also 5:00 PM to 7:30 PM Thursday evening (no appointment necessary.) Building Code information is also available online at: <http://www.portlandonline.com/bds/>, or by calling (503) 823-1456.
- It appears that the sidewalk slopes as a contour, parallel to the front of the building. Landings to enter the spaces may be required. If the slope of the sidewalk between tenant door landings exceeds 1:20 a handrail is required on both sides. More information regarding Accessibility requirements can be obtained by visiting the Bureau of Development Services Development Services Center -1900 SW 4th Ave, 1st floor, between the hours of 7:30 AM and 3:00 PM Monday through Friday and also 5:00 PM to 7:30 PM Thursday evening (no appointment necessary.) Building Code information is also available online at: <http://www.portlandonline.com/bds/>.

The Bureau of Transportation Engineering responded with the following comment:

- Provided that IBC standards for encroachments into the public right-of-way are met, Transportation has no objection to this proposal.

Neighborhood Review: A Notice of Proposal in Your Neighborhood was mailed on June 30, 2008. No written responses have been received from either the Neighborhood Association or notified property owners in response to the proposal.

ZONING CODE APPROVAL CRITERIA

Chapter 33.825 Design Review

Section 33.825.010 Purpose of Design Review

Design review ensures that development conserves and enhances the recognized special design values of a site or area. Design review is used to ensure the conservation, enhancement, and continued vitality of the identified scenic, architectural, and cultural values of each design district or area. Design review ensures that certain types of infill development will be compatible with the neighborhood and enhance the area. Design review is also used in certain cases to review public and private projects to ensure that they are of a high design quality.

Section 33.825.055 Design Review Approval Criteria

A design review application will be approved if the review body finds the applicant to have shown that the proposal complies with the design guidelines for the area.

Findings: The site is designated with design overlay zoning (d), therefore the proposal requires Design Review approval. Because of the site's location, the applicable design guidelines are the Central City Fundamental Design Guidelines.

Central City Fundamental Design Guidelines

These guidelines provide the constitutional framework for all design review areas in the Central

City.

The Central City Fundamental Design Guidelines and the River District Design Guidelines focus on four general categories. **(A) Portland Personality**, addresses design issues and elements that reinforce and enhance Portland's character. **(B) Pedestrian Emphasis**, addresses design issues and elements that contribute to a successful pedestrian environment. **(C) Project Design**, addresses specific building characteristics and their relationships to the public environment. **(D) Special Areas**, provides design guidelines for the four special areas of the Central City.

Central City Plan Design Goals

This set of goals are those developed to guide development throughout the Central City. They apply within the River District as well as to the other seven Central City policy areas. The nine goals for design review within the Central City are as follows:

1. Encourage urban design excellence in the Central City;
2. Integrate urban design and preservation of our heritage into the development process;
3. Enhance the character of the Central City's districts;
4. Promote the development of diversity and areas of special character within the Central City;
5. Establish an urban design relationship between the Central City's districts and the Central City as a whole;
6. Provide for a pleasant, rich and diverse pedestrian experience for pedestrians;
7. Provide for the humanization of the Central City through promotion of the arts;
8. Assist in creating a 24-hour Central City which is safe, humane and prosperous;
9. Ensure that new development is at a human scale and that it relates to the scale and desired character of its setting and the Central City as a whole.

Staff has considered all guidelines and has addressed only those guidelines considered applicable to this project.

A4. Use Unifying Elements. Integrate unifying elements and/or develop new features that help unify and connect individual buildings and different areas.

A5. Enhance, Embellish, and Identify Areas. Enhance an area by reflecting the local character within the right-of-way. Embellish an area by integrating elements in new development that build on the area's character. Identify an area's special features or qualities by integrating them into new development.

Findings: The proposed façade is more consistent with the original storefront on the building and reminiscent of traditional storefronts within the Central City District. The remodel that occurred in 1989 removed a large portion of the original storefront including glazing and replaced it with solid stucco walls. The proposal recreates six missing pilasters that define the individual bays and which line up with the upper floor window pattern. It also re-introduces transom windows above the storefront which exists in the western two bays on the Salmon façade. The full height of the storefront is restored within the new system with a base, storefront glass, transom and upper transom windows. Each pedestrian door is set within the recessed entry. *These guidelines are therefore met.*

A8. Contribute to a Vibrant Streetscape. Integrate building setbacks with adjacent sidewalks to increase the space for potential public use. Develop visual and physical connections into buildings' active interior spaces from adjacent sidewalks. Use architectural elements such as atriums, grand entries and large ground-level windows to reveal important interior spaces and activities.

B2. Protect the Pedestrian. Protect the pedestrian environment from vehicular movement. Develop integrated identification, sign, and sidewalk-oriented night-lighting systems that offer safety, interest, and diversity to the pedestrian. Incorporate building equipment, mechanical exhaust routing systems, and/or service areas in a manner that does not detract from the pedestrian environment.

C1. Enhance View Opportunities. Orient windows, entrances, balconies and other

building elements to surrounding points of interest and activity. Size and place new buildings to protect existing views and view corridors. Develop building façades that create visual connections to adjacent public spaces.

C12. Integrate Exterior Lighting. Integrate exterior lighting and its staging or structural components with the building's overall design concept. Use exterior lighting to highlight the building's architecture, being sensitive to its impacts on the skyline at night.

Findings: The remodel replaces the solid stucco walls added in the late 1980's with clear glazing which reinforces the pedestrian system and allows for additional views into retail spaces and connects them with the pedestrian environment. The placement of the louvered vents within the transom system and within the ceiling of the alcoves limits their visibility and allows for their removal in the future and transom glass replacement. The only building lighting proposed is within the soffits of the recessed entries and the two alcoves. This type of down lighting will illuminate the building's access points for pedestrian safety without any impacts on the nighttime skyline. The recessed entries within each the storefront bays will also provide weather protection for pedestrians. *These guidelines are therefore met.*

C2. Promote Quality and Permanence in Development. Use design principles and building materials that promote quality and permanence.

Findings: The brick and cast stone used to rebuild the columns along the storefront are both permanent and high quality materials. The black aluminum storefront framing is also a durable and long-lasting material that requires little maintenance. *This guideline is therefore met.*

C3. Respect Architectural Integrity. Respect the original character of an existing building when modifying its exterior. Develop vertical and horizontal additions that are compatible with the existing building, to enhance the overall proposal's architectural integrity.

C4. Complement the Context of Existing Buildings. Complement the context of existing buildings by using and adding to the local design vocabulary.

C5. Design for Coherency. Integrate the different building and design elements including, but not limited to, construction materials, roofs, entrances, as well as window, door, sign, and lighting systems, to achieve a coherent composition.

Findings: The proposed remodel is consistent with the original storefront of this building thus maintaining its character and provides a uniform system resulting in a coherent composition. The re-creation of the missing pilasters along both facades align with the window pattern on the upper floor and reinforce the modules of the building and the individual bays. The cast stone base and brick column match the existing pilasters as does the brick detailing and 8" projection from the building's façade.

Although a modern system, the aluminum storefront will provide details that complement the upper windows and an traditional storefront. The system will reintroduce transom windows throughout the ground floor that currently only exist on a portion of the Salmon façade. The system will provide a base with a metal faced composite panel proud of the glazing and include ½" trim around the infill panel. The trim provides articulation to the typically flat modern system and adds mass to the base that is more common in older storefronts. The decorative fascia on the transom also incorporates more traditional detailing. The recessed glazing on the storefront and detailed base and transom fascia create depth and shadow lines that are harmonious with the original windows to remain on the upper façade.

The aluminum louvered vent will be surrounded by a 1" wide frame and sit flush with the glass in the upper transom. The style and installation of the vents will

complement the storefront system and be uniform within the transom system.
These guidelines are therefore met.

C7. Design Corners that Build Active Intersections. Use design elements including, but not limited to, varying building heights, changes in façade plane, large windows, awnings, canopies, marquees, signs and pedestrian entrances to highlight building corners. Locate flexible sidewalk-level retail opportunities at building corners. Locate stairs, elevators, and other upper floor building access points toward the middle of the block.

Findings: The proposed remodel adds larger windows to the storefront and provides a continuous transparency along the street-facing facade that wraps the corner of SW Broadway and SW Salmon. The views into and out of the corner bays, as well as adjacent main entrances to retail tenants, support and reinforce active intersections. *This guideline is therefore met.*

C8. Differentiate the Sidewalk-Level of Buildings. Differentiate the sidewalk-level of the building from the middle and top by using elements including, but not limited to, different exterior materials, awnings, signs, and large windows.

Findings: The restoration of storefront provides continuous glazing along the ground level facades further defining the building's base and clearly distinguishing the lower retail façade from the upper elevation. The black aluminum material used throughout the new storefront system will also provide a consistent composition throughout the ground level which is currently comprised of wood, aluminum and stucco storefront. *This guideline is therefore met.*

DEVELOPMENT STANDARDS

Unless specifically required in the approval criteria listed above, this proposal does not have to meet the development standards in order to be approved during this review process. The plans submitted for a building or zoning permit must demonstrate that all development standards of Title 33 can be met, or have received an Adjustment or Modification via a land use review prior to the approval of a building or zoning permit.

CONCLUSIONS

The design review process exists to promote the conservation, enhancement, and continued vitality of areas of the City with special scenic, architectural, or cultural value. The proposal restores the composition of the original storefront which is more consistent with the building and with traditional commercial storefronts. The proposal meets the applicable design guidelines and therefore warrants approval.

ADMINISTRATIVE DECISION

Approval of storefront remodel per the approved site plans, Exhibits C-1 through C-8, signed and dated 7/24/08, subject to the following conditions:

- A. As part of the building permit application submittal, each of the 4 required site plans and any additional drawings must reflect the information and design approved by this land use review as indicated in Exhibits C.1-C.8. The sheets on which this information appears must be labeled, "Proposal and design as approved in Case File # LU 08-138580 DZ. No field changes allowed."

Decision rendered by:  **on July 24, 2008**

By authority of the Director of the Bureau of Development Services

Decision mailed: July 28, 2008

Staff Planner: Staci Monroe

About this Decision. This land use decision is **not a permit** for development. Permits may be required prior to any work. Contact the Development Services Center at 503-823-7310 for information about permits.

Procedural Information. The application for this land use review was submitted on June 18, 2008, and was determined to be complete on June 26, 2008.

Zoning Code Section 33.700.080 states that Land Use Review applications are reviewed under the regulations in effect at the time the application was submitted, provided that the application is complete at the time of submittal, or complete within 180 days. Therefore this application was reviewed against the Zoning Code in effect on June 18, 2008.

ORS 227.178 states the City must issue a final decision on Land Use Review applications within 120-days of the application being deemed complete. The 120-day review period may be waived or extended at the request of the applicant. In this case, the applicant did not waive or extend the 120-day review period.

Some of the information contained in this report was provided by the applicant.

As required by Section 33.800.060 of the Portland Zoning Code, the burden of proof is on the applicant to show that the approval criteria are met. The Bureau of Development Services has independently reviewed the information submitted by the applicant and has included this information only where the Bureau of Development Services has determined the information satisfactorily demonstrates compliance with the applicable approval criteria. This report is the decision of the Bureau of Development Services with input from other City and public agencies.

Conditions of Approval. If approved, this project may be subject to a number of specific conditions, listed above. Compliance with the applicable conditions of approval must be documented in all related permit applications. Plans and drawings submitted during the permitting process must illustrate how applicable conditions of approval are met. Any project elements that are specifically required by conditions of approval must be shown on the plans, and labeled as such.

These conditions of approval run with the land, unless modified by future land use reviews. As used in the conditions, the term "applicant" includes the applicant for this land use review, any person undertaking development pursuant to this land use review, the proprietor of the use or development approved by this land use review, and the current owner and future owners of the property subject to this land use review.

Appealing this decision. This decision may be appealed to the Design Commission, which will hold a public hearing. Appeals must be filed **by 4:30 PM on August 11, 2008** at 1900 SW Fourth Ave. Appeals can be filed on the first floor in the Development Services Center until 3 p.m. After 3 p.m., appeals must be submitted to the receptionist at the front desk on the fifth floor. **An appeal fee of \$250 will be charged.** The appeal fee will be refunded if the appellant prevails. There is no fee for ONI recognized organizations appealing a land use decision for property within the organization's boundaries. The vote to appeal must be in accordance with the organization's bylaws. Low-income individuals appealing a decision for their personal residence that they own in whole or in part may qualify for an appeal fee waiver. In addition, an appeal fee may be waived for a low income individual if the individual resides within the required notification area for the review, and the individual has resided at that address for at least 60 days. Assistance in filing the appeal and information on fee waivers is available from BDS in the Development Services Center. Fee waivers for low-income individuals must be approved prior to filing the appeal; please allow 3 working days for fee waiver approval. Please see the appeal form for additional information.

The file and all evidence on this case are available for your review by appointment only. Please contact the receptionist at 503-823-7967 to schedule an appointment. I can provide some information over the phone. Copies of all information in the file can be obtained for a fee equal to the cost of services. Additional information about the City of Portland, city bureaus, and a digital copy of the Portland Zoning Code is available on the internet at www.portlandonline.com.

Attending the hearing. If this decision is appealed, a hearing will be scheduled, and you will be notified of the date and time of the hearing. The decision of the Design Commission is final; any further appeal must be made to the Oregon Land Use Board of Appeals (LUBA) within 21 days of the date of mailing the decision, pursuant to ORS 197.620 and 197.830. Contact LUBA at 550 Capitol St. NE, Suite 235, Salem, Oregon 97301, or phone 1-503-373-1265 for further information.

Failure to raise an issue by the close of the record at or following the final hearing on this case, in person or by letter, may preclude an appeal to the Land Use Board of Appeals (LUBA) on that issue. Also, if you do not raise an issue with enough specificity to give the Design Commission an opportunity to respond to it, that also may preclude an appeal to LUBA on that issue.

Recording the final decision.

If this Land Use Review is approved the final decision must be recorded with the Multnomah County Recorder. A few days prior to the last day to appeal, the City will mail instructions to the applicant for recording the documents associated with their final land use decision.

- *Unless appealed*, The final decision may be recorded on or after **August 12, 2008 – (the day following the last day to appeal)**.
- A building or zoning permit will be issued only after the final decision is recorded.

The applicant, builder, or a representative may record the final decision as follows:

- **By Mail:** Send the two recording sheets (sent in separate mailing) and the final Land Use Review decision with a check made payable to the Multnomah County Recorder to: Multnomah County Recorder, P.O. Box 5007, Portland OR 97208. The recording fee is identified on the recording sheet. Please include a self-addressed, stamped envelope.
- **In Person:** Bring the two recording sheets (sent in separate mailing) and the final Land Use Review decision with a check made payable to the Multnomah County Recorder to the County Recorder's office located at 501 SE Hawthorne Boulevard, #158, Portland OR 97214. The recording fee is identified on the recording sheet.

For further information on recording, please call the County Recorder at 503-988-3034
For further information on your recording documents please call the Bureau of Development Services Land Use Services Division at 503-823-0625.

Expiration of this approval. An approval expires three years from the date the final decision is rendered unless a building permit has been issued, or the approved activity has begun.

Where a site has received approval for multiple developments, and a building permit is not issued for all of the approved development within three years of the date of the final decision, a new land use review will be required before a permit will be issued for the remaining development, subject to the Zoning Code in effect at that time.

Zone Change and Comprehensive Plan Map Amendment approvals do not expire.

Applying for your permits. A building permit, occupancy permit, or development permit may be required before carrying out an approved project. At the time they apply for a permit, permittees must demonstrate compliance with:

- All conditions imposed herein;

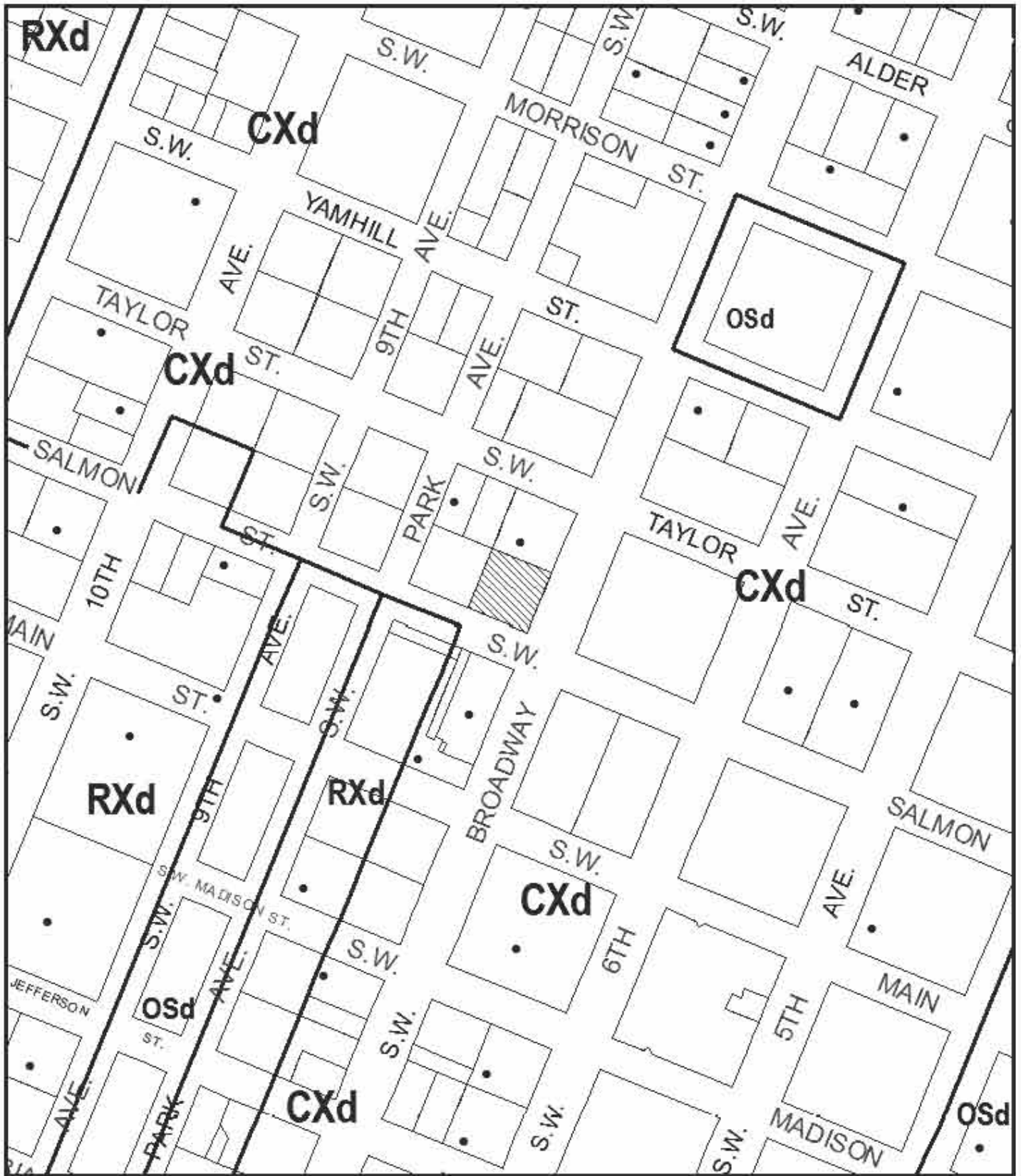
- All applicable development standards, unless specifically exempted as part of this land use review;
- All requirements of the building code; and
- All provisions of the Municipal Code of the City of Portland, and all other applicable ordinances, provisions and regulations of the City.

EXHIBITS

NOT ATTACHED UNLESS INDICATED


- A. Applicant's Statement
- B. Zoning Map (attached)
- C. Plans/Drawings:
 - 1. Site Plan (attached)
 - 2. Salmon Street Elevation (attached)
 - 3. Broadway Avenue Elevation (attached)
 - 4. 1st Floor Plan
 - 5. Transom Cross-Section/Details
 - 6. Storefront Cross-Section/Details
 - 7. Louvered Vent Detail
 - 8. Column Detail
- D. Notification information:
 - 1. Mailing list
 - 2. Mailed notice
- E. Agency Responses:
 - 1. Plan Review Section of BDS
- F. Correspondence: none
- G. Other:
 - 1. Original LU Application
 - 2. Site History Research

The Bureau of Development Services is committed to providing equal access to information and hearings. If you need special accommodations, please call 503-823-0625 (TTY 503-823-6868).



ZONING

 Site

 Historic Landmark



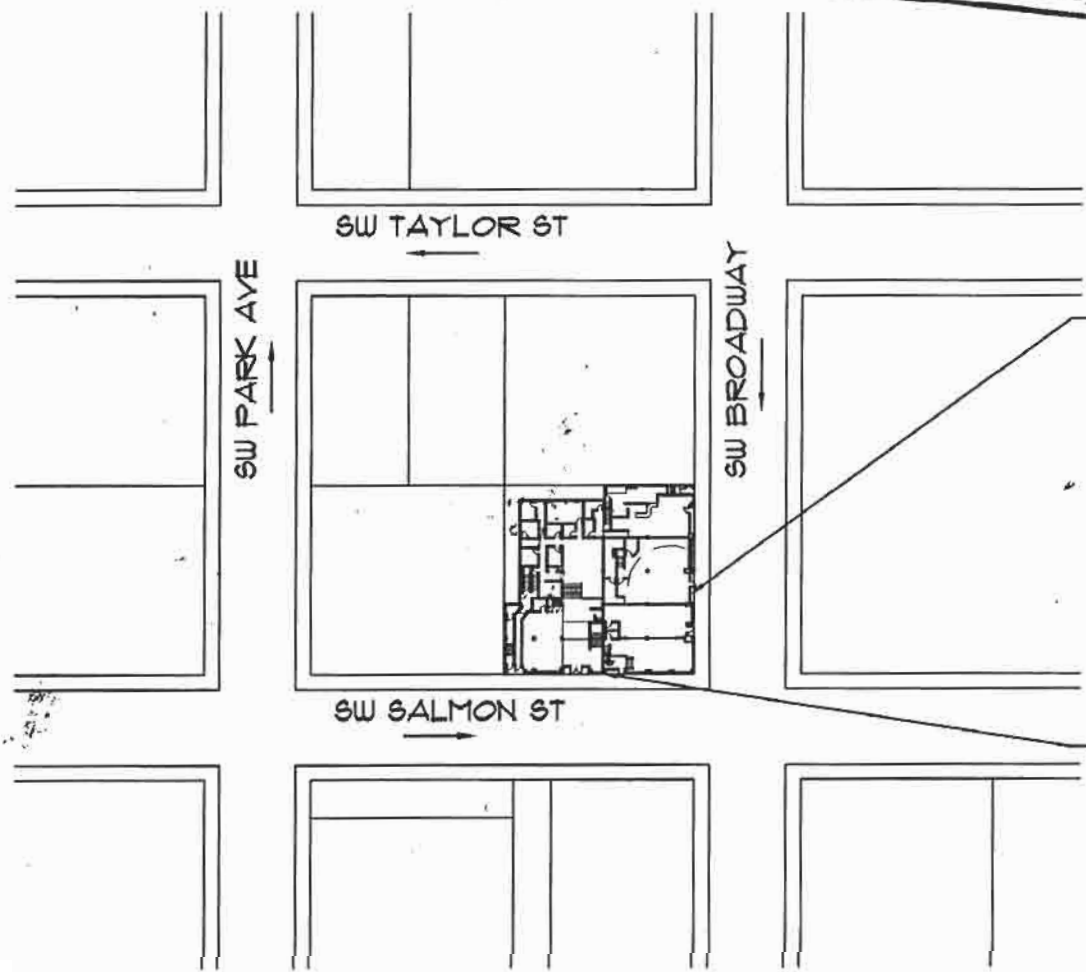
This site lies within the:
CENTRAL CITY PLAN DISTRICT

File No.	LU 08-138580 DZ
1/4 Section	3129
Scale	1 inch = 200 feet
State_Id	1S1E03BB 3900
Exhibit	B (Jun 23, 2008)

Approved
 City of Portland - Bureau of Development Services

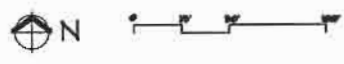
Planner SWL Date 7/24/08

* This approval applies only to the reviews requested and is subject to all conditions of approval. Additional zoning requirements may apply.



ADD NEW ALUMINUM STOREFRONT SYSTEM, REBUILD MASONRY COLUMNS INCLUDING CAPITALS AND BASES, CLEAN PATCH AND RE-PAINT EXISTING WOOD WINDOWS, PAINT ENTIRE BUILDING (SEE NEW BUILDING ELEVATIONS, SHEET D-R 2)

ADD NEW ALUMINUM STOREFRONT SYSTEM, REBUILD MASONRY COLUMNS INCLUDING CAPITALS AND BASES, CLEAN PATCH AND RE-PAINT EXISTING WOOD WINDOWS, PAINT ENTIRE BUILDING (SEE NEW BUILDING ELEVATIONS, SHEET D-R 2)



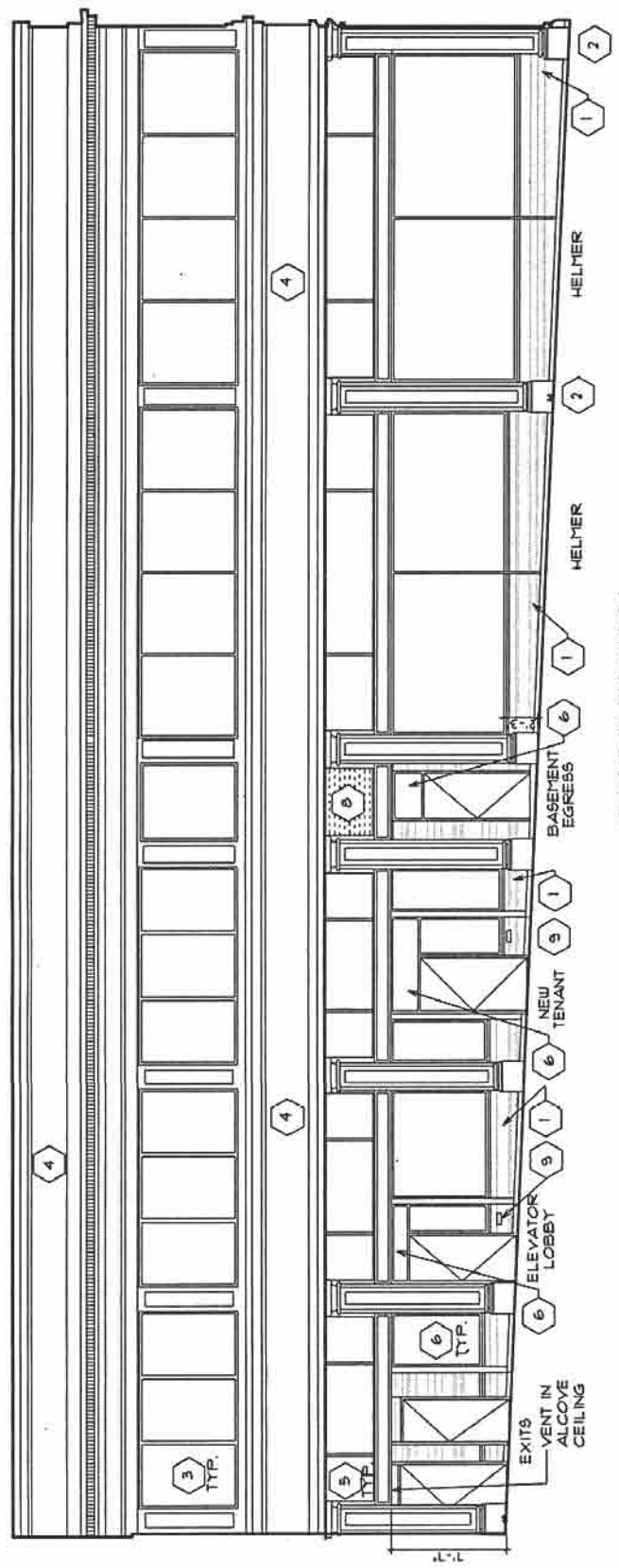
①

WESTERN BUILDING - SITE PLAN

08-138580 02
 EX C-1

Approved
 City of Portland - Bureau of Development Services
 Planner Sullen Date 7/24/08
 * This approval applies only to the reviews requested and is subject to all conditions of approval. Additional zoning requirements may apply.

ARON FAEGRE ARCHITECT 200 1st NW PORTLAND, OREGON 97208 503-228-2648	WESTERN BUILDING DOWNTOWN DEVELOPMENT GROUP LLC, PORTLAND, OREGON	SALMON ELEVATION REVISED WESTERN BUILDING	NO. DATE DESCRIPTION 1 7-24-08	REVISIONS	PAGE: A2.1
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VENTS THIS ELEVATION
 1 TRANSOM VENT
 1 ALCOVE CEILING VENT

1 SALMON STREET ELEVATION - NEW
 SCALE: 1/8" = 1'-0"

- 1 NEW BULKHEAD WALL BELOW STOREFRONT SYSTEM WITH SOLID INFILL PANELS
 - 2 REBUILD MASONRY COLUMNS INCLUDING CAPITALS AND BASES
 - 3 CLEAN PATCH AND REPAIR EXISTING WOOD WINDOWS
 - 4 PAINT ENTIRE BUILDING
 - 5 TRANSOM GLASS
 - 6 NEW ALUMINUM STOREFRONT SYSTEM
 - 7 NOT USED
 - 8 HVAC LOUVER
 - 9 MAIL SLOT
 - SOLID INFILL PANELS
- BLACK

NOTE:
 FOR SIGNAGE REFER TO PROPOSED RENDERING OF BUILDING

08-138580 02
 EX. C-2

* Approved*
 City of Portland - Bureau of Development Services
 Planner SWL Date 7/24/08
 * This approval applies only to the reviews requested and is subject to all conditions of approval. Additional zoning requirements may apply.

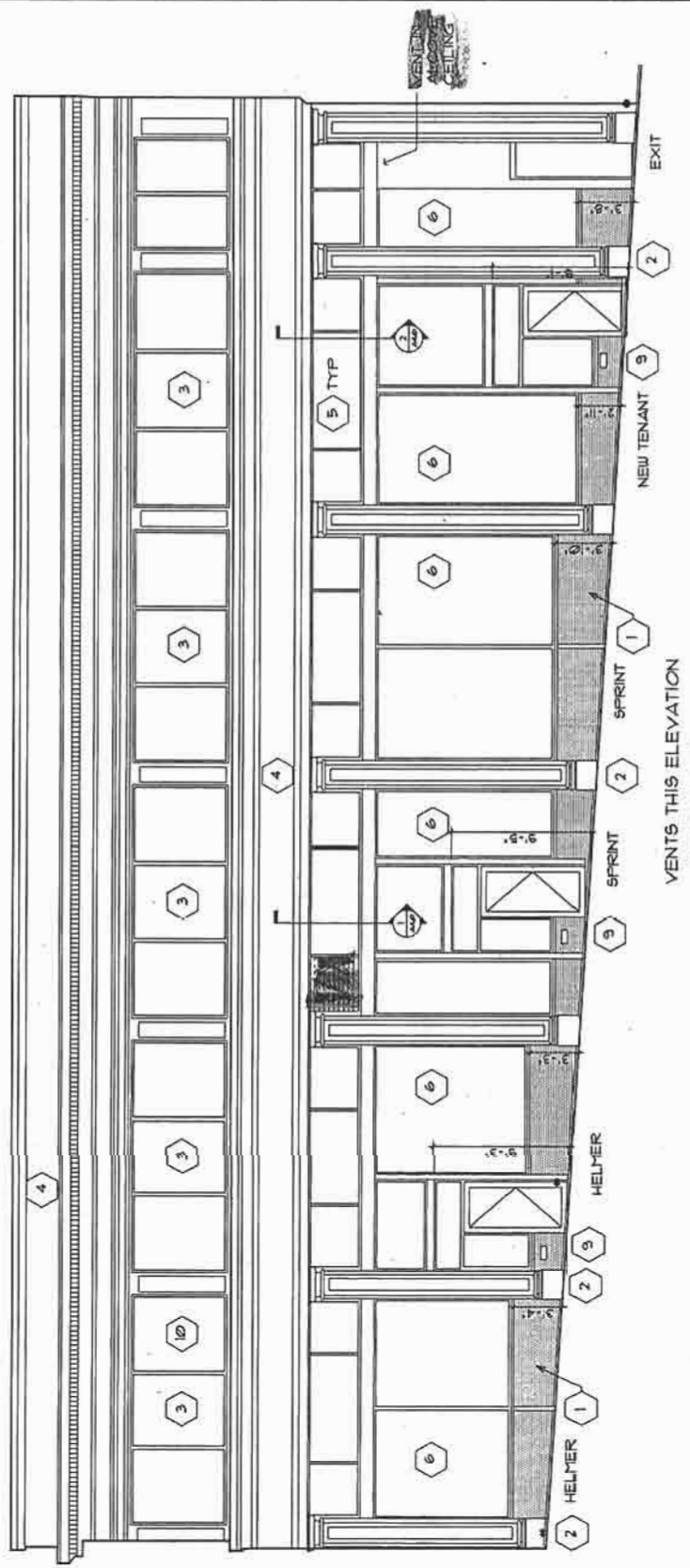
ARON FAEGRE ARCHITECT
 500 SW VAN HELL
 PORTLAND OREGON 97204
 503-222-2548

WESTERN BUILDING
 DOWNTOWN DEVELOPMENT GROUP LLC, PORTLAND, OREGON

BROADWAY ELEVATION
 DATE: 7/24/08
 DRAWN BY: [Signature]

NO.	REVISIONS	DATE	BY

PAGE: **A3.1**



1 BROADWAY ELEVATION - NEW
 SCALE: 1/8" = 1'-0"

- 1 NEW BULK-HEAD WALL BELOW STOREFRONT SYSTEM WITH SOLID INFILL PANELS
 - 2 REBUILD MASONRY COLUMNS INCLUDING CAPITALS AND BASES
 - 3 CLEAN PATCH AND REPAINT EXISTING WOOD WINDOWS
 - 4 PAINT ENTIRE BUILDING
 - 5 TRANSOM GLASS
 - 6 NEW ALUMINUM STOREFRONT SYSTEM
 - 7 NOT SHOWN
 - 8 HVAC LOUVER
 - 9 MAIL SLOT
 - 10 REPLACE ALUM. WINDOW W/ WOOD WINDOWS TO MATCH ADJ.
- SOLID INFILL PANELS
- NOTE: FOR SIGNAGE REFER TO PROPOSED RENDERING OF BUILDING

BACK

08-13856002
 EXC-3