



City of Portland, Oregon
Bureau of Development Services
Land Use Services

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Date: January 28, 2009
To: Interested Person
From: Dave Skilton, Land Use Services
503-823-0660 / dave.skilton@ci.portland.or.us

NOTICE OF A TYPE II DECISION ON A PROPOSAL IN YOUR NEIGHBORHOOD

The Bureau of Development Services has approved a proposal in your neighborhood. The reasons for the decision are included in this notice. If you disagree with the decision, you can appeal it and request a public hearing. Information on how to appeal this decision is listed at the end of this notice.

CASE FILE NUMBER: LU 08-190258 HDZ – CLIFFORD APARTMENTS REHABILITATION

GENERAL INFORMATION

Applicant: Clifford Investments LLC, Owner
1214 SW Washington Street
Portland, OR 97205

Julie Garver, Manager
Innovative Housing Inc.
1214 SW Washington Street
Portland, OR 97205

Representative: Paul Falsetto, Architect (503-243-2252)
Carlton Hart Architecture, PC
322 NW 8th Avenue
Portland, OR 97209

Site Address: 535 SE Morrison Street

Legal Description: EXC S 12' IN E MORRISON ST LOT 5 BLOCK 124 LOT 6 BLOCK 124, EAST PORTLAND

Tax Account No.: R226508470
State ID No.: 1S1E02BB 04900
Quarter Section: 2826

Neighborhood: Buckman, contact Susan Lindsay at 503-725-8257.
Business District: Central Eastside Industrial Council, contact Chris Hammond at 503-309-1971.
District Coalition: Southeast Uplift, contact Cece Hughley Noel at 503-232-0010.
Plan District: Central City, Central Eastside Subdistrict

Other Designations: Contributing resource in East Portland Grand Avenue Historic District, pursuant to listing in the National Register of Historic Places on March 4, 1991.

Zoning: EXd, Central Employment with Design Overlay

Case Type: HDZ , Historic Design Review

Procedure: Type II, an administrative decision with appeal to the Historic Landmarks Commission.

Proposal:

Applicant is seeking Historic Design Review for proposed rehabilitation work on the Clifford Hotel building, a contributing resource in the East Portland Grand Avenue Historic District. The proposal includes:

- replacement of non-historic windows on the south and east sides of the building with fiberglass, one-over-one, single-hung units;
- reconfiguration of the main entrance façade facing SE Morrison, including a marquise style canopy and new light fixtures;
- re-pointing and re-construction of the brick masonry at parapet level; and
- installation of rooftop mechanical equipment

Historic Design Review is required because the property is within the East Portland Grand Avenue Historic District, listed in the National Register of Historic Places on March 4, 1991.

Relevant Approval Criteria:

In order to be approved, this proposal must comply with the approval criteria of Title 33, Portland Zoning Code. The relevant criteria are:

- East Portland Grand Avenue Historic District Design Guidelines
- Central City Fundamental Design Guidelines

ANALYSIS

Site and Vicinity: The 8,800 square-foot site is located at the corner of SE 6th Avenue and SE Morrison Street in the Central Eastside Sub-District of the Central City Plan District. The site is developed with the Clifford Hotel building, a contributing structure in the East Portland/Grand Avenue Historic District. This four-story building was built in 1911 and is constructed of reinforced concrete faced with tawny-colored brick.

The building's history includes numerous episodes of remodeling of its storefront system, including removal of both the original and a later canopy. In the late 1920s a section of the southerly part of the building twelve feet deep was removed to accommodate the widening of Morrison Street for a streetcar line. The current façade was built at that time. A particularly damaging fire occurred in the 1990s, requiring substantial repair work at the east wing on floors two through four and replacement of that wing's entire roof system.

The East Portland/Grand Avenue Historic District is an approximately 20-block area. The historic district represents the early origins of the City of East Portland and its subsequent development as the major commercial core of eastside Portland. The historic period of significance is 1883-1930. The historic district is made up of 61% primary and secondary significant buildings excluding vacant land. Eighty-nine percent of the buildings were constructed prior to 1930.

Portland's *Transportation System Plan* classifies SE Morrison Street as a Major City Traffic Street, Major Transit Priority Street, Local Service Bikeway and City Walkway. SE 6th Avenue is designated a Local Service Bikeway and Local Service Walkway. Tri-Met bus route 15 operates on SE Morrison Street.

Zoning: The Central Employment (EX) zone allows mixed-uses and is intended for areas in the center of the City that have predominantly industrial type development. The intent of the zone is to allow industrial and commercial uses which need a central location. Residential uses are allowed, but are not intended to predominate or set development standards for other uses in the area.

The design (d) overlay zone promotes the conservation, enhancement, and continued vitality of areas of the City with special scenic, architectural, or cultural value. This is achieved through the creation of design districts and applying the Design Overlay Zone as part of community planning projects, development of design guidelines for each district, and by requiring design review. In addition, design review ensures that certain types of infill development will be compatible with the neighborhood and enhance the area.

Land Use History: City records indicate that prior land use reviews include the following:

- LUR 00-007183 HDZ (reference file #00-00628): Approval of alterations to two ground floor entries, including the new trash room door on the east elevation and the apartment lobby entrance on the south elevation; replacement of two retail storefront doors on the south elevation with black anodized aluminum, replacing non-original uncolored aluminum window system with wood transom lights replaced with the same system; and aluminum projecting signs for each tenant space.

Agency Review: A *Notice of Proposal in Your Neighborhood* was mailed on December 23, 2008. The following Bureaus have responded with no issues or concerns:

- Portland Fire & Rescue;
- Urban Forestry Division of Portland Parks & Recreation;
- Site Development Section of the Bureau of Development Services.
- The Development Review Section of the Bureau of Transportation responded in TRACS with the following comment: *Transportation has no objection provided that IBC standards are met for modified door swings into public right-of-way.*
- The Bureau of Environmental Services responded with the following comment: *BES has no objections to the proposed exterior upgrades to the Clifford Hotel Building. Further development of the property would be subject to the Bureau of Environmental Services' standards and requirements during the building plan review process. Please see Exhibit E.1 for additional details.*
- The Portland Water Bureau responded with the following comment: *The Water Bureau has no objections to the proposed action. Please see Exhibit E.2 for additional details.*
- The Life Safety (Building Code) Section of the Bureau of Development Services responded with the following comment: *Based on the information provided, there appears to be no conflicts between the proposal and applicable building codes, for the purpose of obtaining a LU approval. A separate Building Permit is required for the work proposed and the proposal must be designed to meet all applicable building codes and ordinances. Please see Exhibit E.3 for additional details.*

Neighborhood Review: A *Notice of Proposal in Your Neighborhood* was mailed on **December 23, 2008**. One written comment in response to the proposal has been received from the neighborhood association or notified property owners.

- On Monday, December 29, 2008 a response was received from Doug Klotz (Exhibit F.1). Mr. Klotz's letter supports the window replacement, the repointing work, and the new canopy, but expresses concern that the proposed asymmetrical arrangement of door and sidelight within the centered main entry is not in character with the building. In addition,

he believes the preservation of the existing hex tile floor should be a priority in this project.

Staff Response: Staff consulted with the architect and determined that the proposed asymmetry of the door and sidelight is related to the mechanism used to make the door operable by wheelchair users. The fact that the entry is centered and marked by a canopy is sufficient to reinforce the symmetry of the overall design. The door and sidelight are recessed, as are other entries into the ground floor, and are reasonably compatible with historic character. The hex tile floor dates to the 1980s and is not subject to Historic Design Review in any event because it is an interior feature of the building.

ZONING CODE APPROVAL CRITERIA

Chapter 33.846, Historic Reviews

Purpose of Historic Design Review

Historic Design Review ensures the conservation and enhancement of the special characteristics of historic resources.

Historic Design Review Approval Criteria

Requests for historic design review will be approved if the review body finds the applicant has shown that all of the approval criteria have been met.

Findings: The site is located within the East Portland/Grand Ave. Historic District. Therefore the proposal requires historic design review approval. The relevant approval criteria are the East Portland Grand Ave. Design Zone Design Guidelines and the Central City Fundamental Design Guidelines.

East Portland/Grand Avenue Historic Design Zone Design Guidelines and Central City Fundamental Design Guidelines

The opportunity exists to reestablish the visual and developmental patterns that supported this District as a bustling, urban environment through sensitively designed infill, new construction and restoration. The goal is to integrate the old with the new to enhance the District's continuity. By building upon the basic design tenets historically established here, yet considering the functional needs of new but compatible uses, this goal can be reached and the future of a vibrant Historic District secured.

The Central City Fundamental Design Guidelines and the River District Design Guidelines focus on four general categories. **(A) Portland Personality**, addresses design issues and elements that reinforce and enhance Portland's character. **(B) Pedestrian Emphasis**, addresses design issues and elements that contribute to a successful pedestrian environment. **(C) Project Design**, addresses specific building characteristics and their relationships to the public environment. **(D) Special Areas**, provides design guidelines for the four special areas of the Central City.

East Portland/Grand Avenue Historic Design Zone Goals

- Encourage urban design excellence in the Central City.
- Integrate urban design and preservation of our heritage into the process of Central City development.
- Enhance the character of Portland's Central City districts.
- Promote the development of diversity and areas of special character within the Central City.
- Establish an urban design relationship between the Central City districts and the Central City as a whole.
- Provide for a pleasant, rich and diverse pedestrian experience in the Central City.
- Provide for the humanization of the Central City through promotion of the arts.
- Assist in creating a 24-hour Central City, which is safe, humane and prosperous.

- Assure that new development is on a human scale and that it relates to the character and scale of the area and the central City.

Central City Plan Design Goals

1. Encourage urban design excellence in the Central City;
2. Integrate urban design and preservation of our heritage into the development process;
3. Enhance the character of the Central City's districts;
4. Promote the development of diversity and areas of special character within the Central City;
5. Establish an urban design relationship between the Central City's districts and the Central City as a whole;
6. Provide for a pleasant, rich and diverse pedestrian experience for pedestrians;
7. Provide for the humanization of the Central City through promotion of the arts;
8. Assist in creating a 24-hour Central City which is safe, humane and prosperous;
9. Ensure that new development is at a human scale and that it relates to the scale and desired character of its setting and the Central City as a whole.

Staff has considered all guidelines and has addressed only those guidelines considered applicable to this project. Central City Fundamental and Historic Eastside/Grand Avenue Guidelines are addressed concurrently.

A4. Use Unifying Elements. Integrate unifying elements and/or develop new features that help unify and connect individual buildings and different areas.

A5. Enhance, Embellish, and Identify Areas. Enhance an area by reflecting the local character within the right-of-way. Embellish an area by integrating elements in new development that build on the area's character. Identify an area's special features or qualities by integrating them into new development.

Findings for A4 and A5: The proposal will help to unify the building's ground level with the historic character of the district by replacing the existing modern storefront system at the building's main residential entry with a more compatible one. A new canopy, derived from photos of the original, and period-derived new wall sconces will also reinforce the primacy of this entrance. *Therefore, these guidelines are met.*

A6. Reuse/Rehabilitate/Restore Buildings. Where practical, reuse, rehabilitate, and restore buildings and/or building elements.

Findings: The existing building will be rehabilitated for continued use as affordable housing. Rehabilitation work includes: repair and repointing of the brick masonry at the parapet level; replacement of non-historic windows with visually compatible fiberglass units; compatible reconfiguration of the building's non-historic main entrance; and installation of new mechanical equipment on the roof. *Therefore, this guideline is met.*

Guidelines for Alterations

Introduction: Scale and Proportion. A building's bulk in conjunction with its proportional height and width will have a significant impact on the degree to which it fits in with its historic neighbors. Structures which are much wider, taller or just are significantly bigger than neighboring structures will disrupt the continuity of the area and may damage the area's historic integrity.

A6-1a. Scale and Proportion

1. The added height or width of an alteration should be compatible with the original scale and proportion first of the affected building and second of adjacent buildings.
2. The scale and proportion of altered or added building elements, such as the size and relationship of new windows, doors, entrances, columns and other building features should be visually compatible with the original architectural character of the building.

3. The visual integrity of the original building should be maintained when altering or adding building elements including the vertical lines of columns, piers, the horizontal definition of spandrels and cornices, and other primary structural and decorative elements.

Findings: The proposal does not include the addition of height or width to the existing building. The new windows and storefront system will fit within original openings, and thus, will not alter the architectural character of the building. *Therefore, this guideline is met.*

Introduction: Exterior Building Materials. After issues of height, mass and bulk the building characteristic having the greatest impact on the District's character will be its exterior materials. Maintaining the integrity of exterior materials is important to protecting the character of the District.

A6-1b. Exterior Building Materials. Exterior surfaces need to be repaired and maintained in a manner that is compatible first with the original building and second with the District.

Findings: The brick masonry at the building's parapet level will be repointed and re-constructed where necessary, employing a mortar formula matching and finish details similar to the original. This will help to maintain the integrity of the resource. *Therefore, this guideline is met.*

Introduction: Rear and Side Walls. Portland and the East Portland/Grand Avenue Historic District share a pattern of orienting corner building entrances to the adjacent north-south street. Within the Historic District city blocks are small resulting in most buildings extending to one or more of the block's corners. Orientations to King Boulevard and Grand Avenue are characteristic of the East Portland/Grand Avenue District. Building alterations should respect this pattern.

Introduction: Exterior Mechanical Systems and Auxiliary Service Elements.

The character of the district may be damaged by the introduction of distracting visual elements which clutter building exteriors. When updating a building's mechanical systems, locating its trash areas and installing electronic communications equipment, care must be taken to avoid visual clutter.

A6-1d. Exterior Mechanical Systems and Auxiliary Service Elements. Avoid unnecessary clutter and unsightliness of mechanical systems, auxiliary structures, and service elements such as trash containers, storage sheds, satellite dishes, etc.

C11. Integrate Roofs and Use Rooftops. Integrate roof function, shape, surface materials, and colors with the building's overall design concept. Size and place rooftop mechanical equipment, penthouses, other components, and related screening elements to enhance views of the Central City's skyline, as well as views from other buildings or vantage points. Develop rooftop terraces, gardens, and associated landscaped areas to be effective stormwater management tools.

Findings for A6-1d and C11: The proposed roof-mounted mechanical equipment will be pulled back from the roof edges and the height of the proposed equipment will not rise above the building's existing parapet. *Therefore, these guidelines are met.*

Introduction: Lighting. Lighting should be compatible with the overall character of the building first and the District second. Lighting should not detract from the character defining features of the building, introduce radically incompatible design elements or damage the building through its installation.

A6-1g. Lighting

1. Repair or replace damaged period lighting with the same or similar lighting fixtures and design elements.
2. Lighting in entryways and doorways and other highly visible public areas should match the original fixtures where possible, or be designed in a manner that is compatible with the historic building and with the district.

3. Bathing a historic building with light or the use of exterior spotlights on the major facade is discouraged, unless historic precedence exists to support such display.

C12. Integrate Exterior Lighting. Integrate exterior lighting and its staging or structural components with the building's overall design concept. Use exterior lighting to highlight the building's architecture, being sensitive to its impacts on the skyline at night.

Findings for A6-1g and C12: Two new period-derived wall scones will be installed on either side of the main residential building entrance. *Therefore, this guideline is met.*

Introduction: Awnings, Canopies, and Marquees. Traditionally, awnings, canopies and marquees were found throughout the District. Awnings were usually sloped at an angle and were attached on the first and second levels of buildings, at entrances, and above storefronts. They should be compatible first with the building and second with the District in size, shape, color, material and overall design. Oversized, inappropriately detailed and shaped awnings, canopies or marquees detract from the historic and architectural character of the building. Though the owner is not required to create an exact duplicate of the historic awning, canopy or marquee, the use of historic photographs of the building or the District that offer precedent for the use and design are highly encouraged in providing a basis to work from.

A6-1h. Awnings, Canopies, and Marquees.

1. Awnings, Canopies, and Marquees should enhance the character-defining features of the original building.
2. Awnings, Canopies, and Marquees should not re-orient the public's perception of the main entry to the building by over emphasizing a minor or secondary facade.
3. Two-story awnings, extremely long horizontal awnings and highly unusually shaped awnings were not part of the traditional character of the District and generally are not in harmony with the District.
4. Awnings should be placed between pilasters or in window bays, not across an entire building, and should be installed below the mezzanine windows.
5. Preferably, awnings and canopies should be made of fabric except when there is historic evidence that the original was made of a material other than fabric. Plastic and aluminum should not be used.
6. Awnings and canopies should be installed in the least destructive manner, generally with a lightweight frame, installed within the window frame. However, it must be recognized that installation of even the most sensitive awning system may result in some damage to the historic fabric. Special care should be taken to avoid harm to decorative features. Retractable awnings are acceptable.
7. Illuminated awnings of a sign-like character are not historic and should be avoided.

Findings: A new, marquise type metal canopy, designed in the spirit of the original as determined through review of historic photographs, will be installed above the existing main residential entrance, approximately 9 feet above the adjacent sidewalk. The canopy will project approximately 6 feet from the face of the building. *Therefore, this guideline is met.*

A7. Establish and Maintain a Sense of Urban Enclosure. Define public rights-of-way by creating and maintaining a sense of urban enclosure.

A8. Contribute to a Vibrant Streetscape. Integrate building setbacks with adjacent sidewalks to increase the space for potential public use. Develop visual and physical connections into buildings' active interior spaces from adjacent sidewalks. Use architectural elements such as atriums, grand entries and large ground-level windows to reveal important interior spaces and activities.

Findings for A7 and A8: The existing four-story building abuts the public sidewalk creating a sense of urban enclosure. The proposed entry canopy will add to this sense of enclosure and will help to highlight the building's main entrance. The reconfigured storefront system will maintain large areas of glazing, which will preserve important visual connections into the main residential lobby. *Therefore, these guidelines are met.*

B1. Reinforce and Enhance the Pedestrian System. Maintain a convenient access route for pedestrian travel where a public right-of-way exists or has existed. Develop and define the different zones of a sidewalk: building frontage zone, street furniture zone, movement zone, and the curb. Develop pedestrian access routes to supplement the public right-of-way system through superblocks or other large blocks.

B2. Protect the Pedestrian. Protect the pedestrian environment from vehicular movement. Develop integrated identification, sign, and sidewalk-oriented night-lighting systems that offer safety, interest, and diversity to the pedestrian. Incorporate building equipment, mechanical exhaust routing systems, and/or service areas in a manner that does not detract from the pedestrian environment.

B6. Develop Weather Protection. Develop integrated weather protection systems at the sidewalk-level of buildings to mitigate the effects of rain, wind, glare, shadow, reflection, and sunlight on the pedestrian environment.

Findings for B1, B2 and B6: The proposal will reinforce the pedestrian system along the site. The new main entry door will be recessed and will open directly onto Morrison Street. The new metal canopy above this entrance will help protect those entering and exiting the building from the elements. In addition, new wall sconces, installed at either side of the main entrance will contribute to the safety of the pedestrian environment in the evening and at night. *Therefore, these guidelines are met.*

B7. Integrate Barrier-Free Design. Integrate access systems for all people with the building's overall design concept.

Findings: The building's main entrance is at grade and a new ADA-compliant ramp will be constructed within the lobby. *Therefore, these guidelines are met.*

C2. Promote Quality and Permanence in Development. Use design principles and building materials that promote quality and permanence.

C3. Respect Architectural Integrity. Respect the original character of an existing building when modifying its exterior. Develop vertical and horizontal additions that are compatible with the existing building, to enhance the overall proposal's architectural integrity.

Findings for C2 and C3: The new entry system will be carefully designed and constructed using high-quality, durable materials. In addition, the proposed residential windows at the upper floors have a more historically accurate profile than the existing non-historic units. The exterior rehabilitation work at the building's parapet will be carried out to match the existing in composition and detailing. *Therefore, these guidelines are met.*

C8. Differentiate the Sidewalk-Level of Buildings. Differentiate the sidewalk-level of the building from the middle and top by using elements including, but not limited to, different exterior materials, awnings, signs, and large windows.

C10. Integrate Encroachments. Size and place encroachments in the public right-of-way to visually and physically enhance the pedestrian environment. Locate permitted skybridges toward the middle of the block, and where they will be physically unobtrusive. Design skybridges to be visually level and transparent.

Findings for C8 and C10: The proposed canopy, which will be installed above the main residential entrance, will help to differentiate the sidewalk-level of the building from its upper levels. In addition, the canopy will be located approximately 9 feet above the sidewalk and will project approximately 6 feet from the face of the building. *Therefore, these guidelines are met.*

DEVELOPMENT STANDARDS

Unless specifically required in the approval criteria listed above, this proposal does not have to meet the development standards in order to be approved during this review process. The plans submitted for a building or zoning permit must demonstrate that all development standards of Title 33 can be met, or have received an Adjustment or Modification via a land use review prior to the approval of a building or zoning permit.


CONCLUSIONS

This modest proposal, to rehabilitate an existing, four story historic apartment building, for continued use as an affordable housing complex, will strengthen the character of the East Portland Grand Avenue Historic District. The repointing and repair of the parapet masonry is especially important because it corrects problems that threaten the structural integrity of the building. The proposal therefore merits approval.

ADMINISTRATIVE DECISION

Approval of rehabilitation work on the historic Clifford Apartments building, per the approved site plans, Exhibits C-1 through C-12, signed and dated January 22, 2009, subject to the following conditions:

- A. As part of the building permit application submittal, each of the 4 required site plans and any additional drawings must reflect the information and design approved by this land use review as indicated in Exhibits C-1 through C-12. The sheets on which this information appears must be labeled, "Proposal and design as approved in Case File # LU 08-190258 HDZ. No field changes allowed."

Decision rendered by:  on January 22, 2009.
By authority of the Director of the Bureau of Development Services

Decision mailed: January 27, 2009

Staff Planner: Dave Skilton

About this Decision. This land use decision is **not a permit** for development. Permits may be required prior to any work. Contact the Development Services Center at 503-823-7310 for information about permits.

Procedural Information. The application for this land use review was submitted on December 12, 2008, and was determined to be complete on **December 19, 2008**.

Zoning Code Section 33.700.080 states that Land Use Review applications are reviewed under the regulations in effect at the time the application was submitted, provided that the application is complete at the time of submittal, or complete within 180 days. Therefore this application was reviewed against the Zoning Code in effect on December 12, 2008.

ORS 227.178 states the City must issue a final decision on Land Use Review applications within 120-days of the application being deemed complete. The 120-day review period may be waived or extended at the request of the applicant. In this case, the applicant did not waive or extend the 120-day review period.

Some of the information contained in this report was provided by the applicant.

As required by Section 33.800.060 of the Portland Zoning Code, the burden of proof is on the applicant to show that the approval criteria are met. The Bureau of Development Services has independently reviewed the information submitted by the applicant and has included this information only where the Bureau of Development Services has determined the information

satisfactorily demonstrates compliance with the applicable approval criteria. This report is the decision of the Bureau of Development Services with input from other City and public agencies.

Conditions of Approval. If approved, this project may be subject to a number of specific conditions, listed above. Compliance with the applicable conditions of approval must be documented in all related permit applications. Plans and drawings submitted during the permitting process must illustrate how applicable conditions of approval are met. Any project elements that are specifically required by conditions of approval must be shown on the plans, and labeled as such.

These conditions of approval run with the land, unless modified by future land use reviews. As used in the conditions, the term “applicant” includes the applicant for this land use review, any person undertaking development pursuant to this land use review, the proprietor of the use or development approved by this land use review, and the current owner and future owners of the property subject to this land use review.

Appealing this decision. This decision may be appealed to the Landmarks Commission, which will hold a public hearing. Appeals must be filed **by 4:30 PM on February 11, 2009** at 1900 SW Fourth Ave. Appeals can be filed on the first floor in the Development Services Center until 3 p.m. After 3 p.m., appeals must be submitted to the receptionist at the front desk on the fifth floor. **An appeal fee of \$250 will be charged.** The appeal fee will be refunded if the appellant prevails. There is no fee for ONI recognized organizations appealing a land use decision for property within the organization’s boundaries. The vote to appeal must be in accordance with the organization’s bylaws. Low-income individuals appealing a decision for their personal residence that they own in whole or in part may qualify for an appeal fee waiver. In addition, an appeal fee may be waived for a low income individual if the individual resides within the required notification area for the review, and the individual has resided at that address for at least 60 days. Assistance in filing the appeal and information on fee waivers is available from BDS in the Development Services Center. Fee waivers for low-income individuals must be approved prior to filing the appeal; please allow 3 working days for fee waiver approval. Please see the appeal form for additional information.

The file and all evidence on this case are available for your review by appointment only. Please contact the receptionist at 503-823-7967 to schedule an appointment. I can provide some information over the phone. Copies of all information in the file can be obtained for a fee equal to the cost of services. Additional information about the City of Portland, city bureaus, and a digital copy of the Portland Zoning Code is available on the internet at www.portlandonline.com.

Attending the hearing. If this decision is appealed, a hearing will be scheduled, and you will be notified of the date and time of the hearing. The decision of the Landmarks Commission is final; any further appeal must be made to the Oregon Land Use Board of Appeals (LUBA) within 21 days of the date of mailing the decision, pursuant to ORS 197.620 and 197.830. Contact LUBA at 550 Capitol St. NE, Suite 235, Salem, Oregon 97301, or phone 1-503-373-1265 for further information.

Failure to raise an issue by the close of the record at or following the final hearing on this case, in person or by letter, may preclude an appeal to the Land Use Board of Appeals (LUBA) on that issue. Also, if you do not raise an issue with enough specificity to give the Landmarks Commission an opportunity to respond to it, that also may preclude an appeal to LUBA on that issue.

Recording the final decision.

If this Land Use Review is approved the final decision must be recorded with the Multnomah County Recorder. A few days prior to the last day to appeal, the City will mail instructions to the applicant for recording the documents associated with their final land use decision.

- *Unless appealed*, The final decision may be recorded on or after **February 12, 2009 – (the day following the last day to appeal)**.
- A building or zoning permit will be issued only after the final decision is recorded.

The applicant, builder, or a representative may record the final decision as follows:

- **By Mail:** Send the two recording sheets (sent in separate mailing) and the final Land Use Review decision with a check made payable to the Multnomah County Recorder to: Multnomah County Recorder, P.O. Box 5007, Portland OR 97208. The recording fee is identified on the recording sheet. Please include a self-addressed, stamped envelope.
- **In Person:** Bring the two recording sheets (sent in separate mailing) and the final Land Use Review decision with a check made payable to the Multnomah County Recorder to the County Recorder's office located at 501 SE Hawthorne Boulevard, #158, Portland OR 97214. The recording fee is identified on the recording sheet.

For further information on recording, please call the County Recorder at 503-988-3034
For further information on your recording documents please call the Bureau of Development Services Land Use Services Division at 503-823-0625.

Expiration of this approval. An approval expires three years from the date the final decision is rendered unless a building permit has been issued, or the approved activity has begun.

Where a site has received approval for multiple developments, and a building permit is not issued for all of the approved development within three years of the date of the final decision, a new land use review will be required before a permit will be issued for the remaining development, subject to the Zoning Code in effect at that time.

Zone Change and Comprehensive Plan Map Amendment approvals do not expire.

Applying for your permits. A building permit, occupancy permit, or development permit may be required before carrying out an approved project. At the time they apply for a permit, permittees must demonstrate compliance with:

- All conditions imposed herein;
- All applicable development standards, unless specifically exempted as part of this land use review;
- All requirements of the building code; and
- All provisions of the Municipal Code of the City of Portland, and all other applicable ordinances, provisions and regulations of the City.

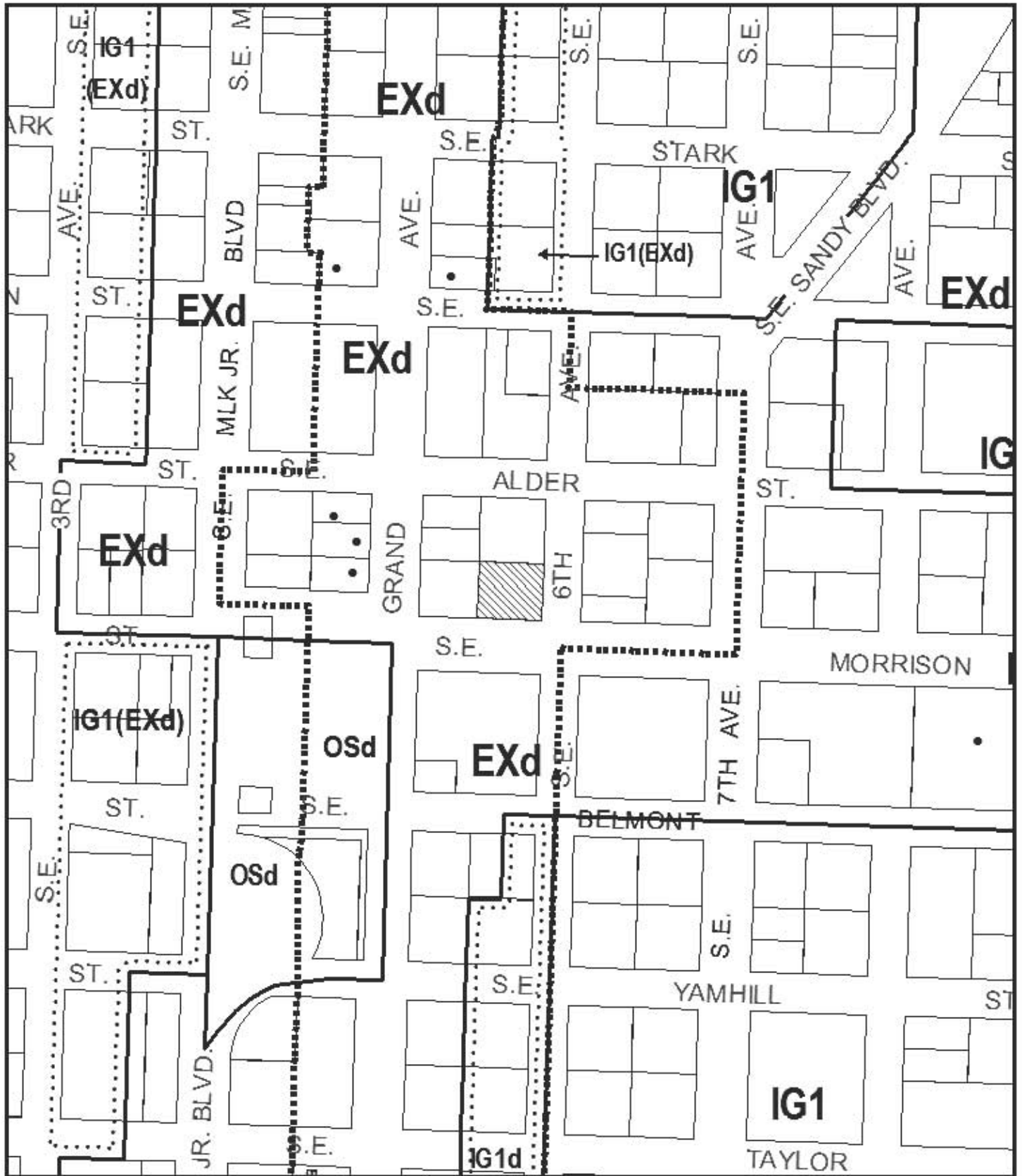
EXHIBITS

NOT ATTACHED UNLESS INDICATED

- A. Applicant's Statement
- B. Zoning Map (attached)
- C. Plans/Drawings:
 - 1. Site Plan (attached)
 - 2. First Floor Plan
 - 3. Roof Plan
 - 4. Enlarged Plans
 - 5. South and East Elevations (attached)
 - 6. North and West Elevations
 - 7. Repointing Details
 - 8. Lobby Elevation and Sections

9. Window Details
 10. Replacement Window Details
 11. Storefront Details
 12. Lighting Fixture Detail
- D. Notification information:
1. Mailing list
 2. Mailed notice
- E. Agency Responses:
1. Bureau of Environmental Services
 2. Bureau of Transportation Engineering and Development Review
 3. Water Bureau
 4. Fire Bureau
 5. Site Development Review Section of BDS
 6. Bureau of Parks, Forestry Division
- F. Correspondence:
1. Doug Klotz, December 29, 2008, concerns regarding asymmetry of proposed entry door and sidelight.
- G. Other:
1. Original LU Application
 2. Site History Research

The Bureau of Development Services is committed to providing equal access to information and hearings. If you need special accommodations, please call 503-823-0625 (TTY 503-823-6868).



ZONING

-  Site
-  Historic Landmark



This site lies within the:
 EAST PORTLAND / GRAND AVENUE HISTORIC DISTRICT HISTORIC DISTRICT
 CENTRAL CITY PLAN DISTRICT

File No.	LU 08-190258 HDZ
1/4 Section	3131
Scale	1 inch = 200 feet
State_Id	1S1E02BB 4900
Exhibit	B (Dec 15, 2008)

PRELIMINARY
NOT FOR
CONSTRUCTION

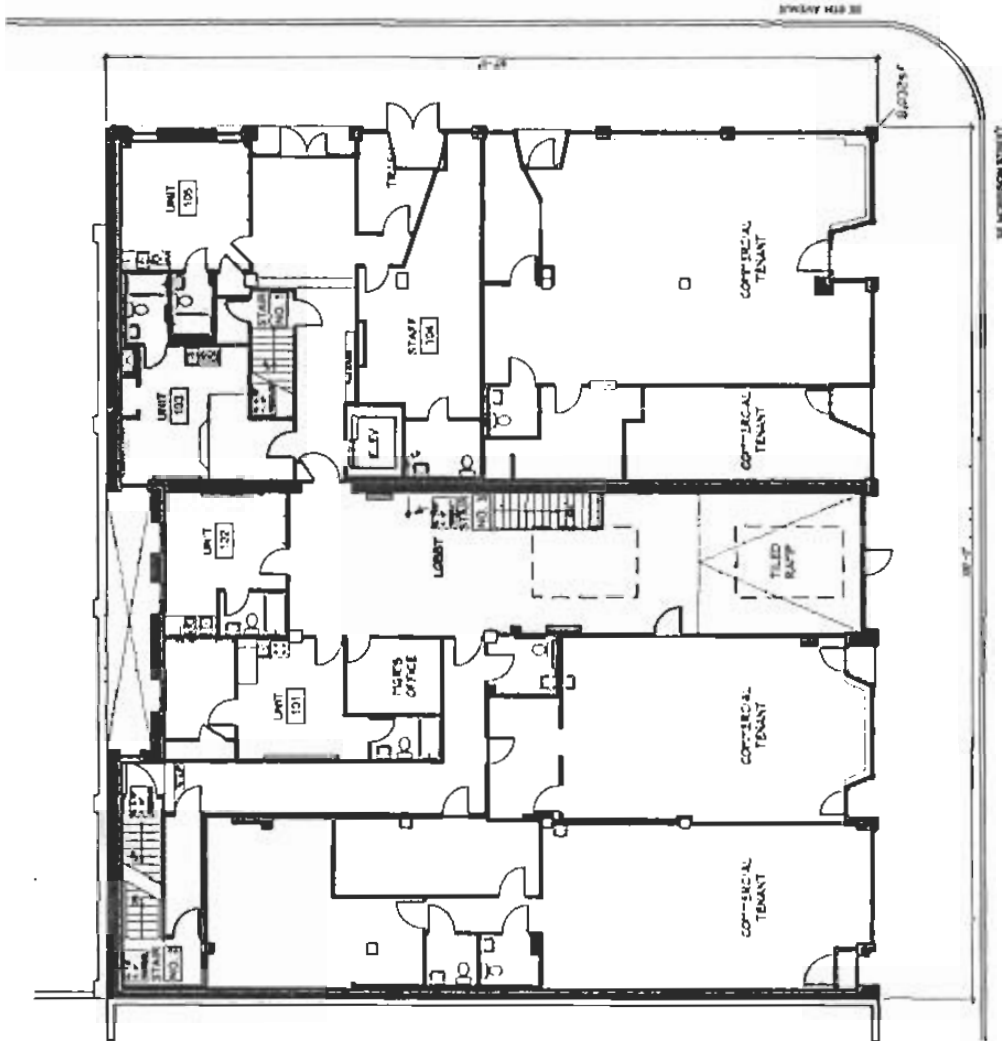
CARLETON HART ARCHITECTURE
1000 SW 10TH AVENUE, SUITE 200, PORTLAND, OREGON 97205
503.228.2222 | carletonhart.com



CLIFFORD APARTMENTS REHABILITATION
INNOVATIVE HOUSING, INC.
527 SE MORRISON STREET | PORTLAND, OR
70% CONSTRUCTION DOCUMENTS

FIRST FLOOR PLAN
- EXISTING
PROJ. NO.
2008.0.01
11.26.08

D2.1



1 SITE AND FIRST FLOOR PLAN - EXISTING
SCALE 1/16" = 1'-0"

LU 08-190258 HDZ

Approved
City of Portland - Bureau of Development Services
Planner Dave Skilton Date 1.23.09
* This approval applies only to the reviews requested and is subject to all conditions of approval. Additional zoning requirements may apply.

LU 08-190258 HDZ
EXHIBIT C-1

PRELIMINARY
NOT FOR
CONSTRUCTION

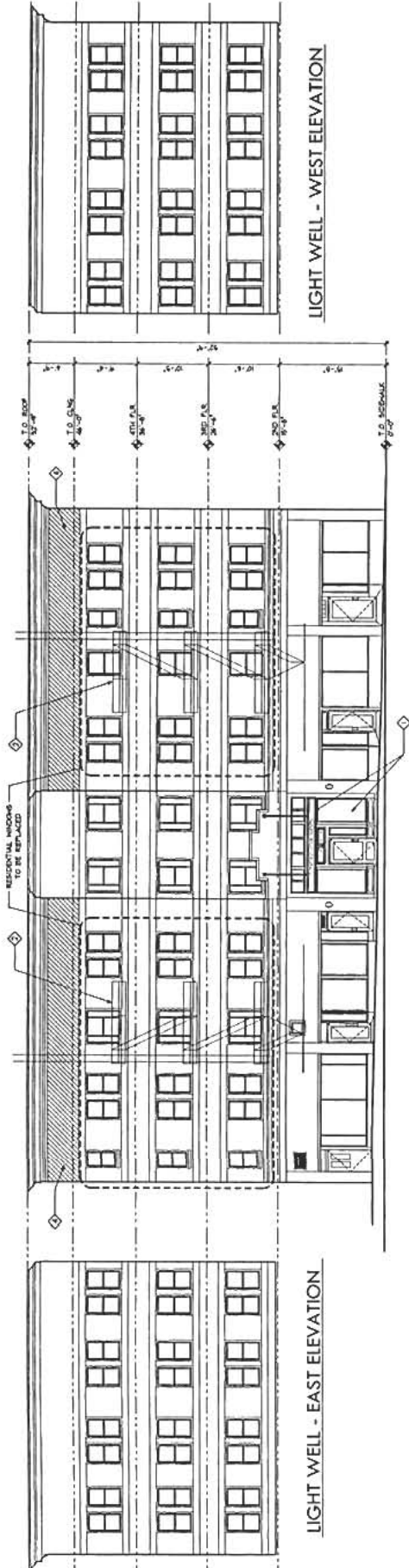


CARLETON HART ARCHITECTURE
1000 NE 10TH AVENUE, SUITE 200
PORTLAND, OREGON 97232 | 503.281.1000

CLIFFORD APARTMENTS REHABILITATION
INNOVATIVE HOUSING, INC.
527 SE MORRISON STREET | PORTLAND, OR
NFS - PART II SUBMITTAL

EXTERIOR ELEVATIONS
- NEW
PROJ. NO.
20803.01
12.17.08

A3.0



1 SOUTH ELEVATION
SCALE 1/8" = 1'-0"

2 EAST ELEVATION
SCALE 1/8" = 1'-0"

ELEVATION GENERAL NOTES	ELEVATION KEY NOTES
<p>ALL RESIDENTIAL WINDOWS ON THE SOUTH & WEST ELEVATIONS TO BE REMOVED AND REPLACED. SEE SIZE AND FOR DETAILS. ALL EXISTING WINDOWS ON THE NORTH, WEST & LIGHT WELL ELEVATIONS TO REMAIN.</p>	<ul style="list-style-type: none"> ◇ HIGH-RISE ESCAPE STATION & GANTRY - SEE 405 FOR DETAILS ◇ FIRE ESCAPE TO REPAIR ◇ HIGH-RYSE INTAKE ◇ AREA OF BRICK REPORTING ◇ LOCATIONS OF UNLIT FLOORS - REMOVE AND POSSIBLE RECONSTRUCTION ◇ AREA OF ANTICIPATED BRICK RECONSTRUCTION

Approved

City of Portland - Bureau of Development Services

Planner Dave Wilson Date 1.23.9

* This approval applies only to the reviews requested and is subject to conditions of approval. Additional zoning requirements may apply.

LU 08-190258 HPZ EXHIBIT C-5