



**City of Portland, Oregon**  
**Bureau of Development Services**  
**Land Use Services**

FROM CONCEPT TO CONSTRUCTION

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**Date:** February 1, 2013  
**To:** Interested Person  
**From:** Kara Fioravanti, Land Use Services  
503-823-5892 / [Kara.Fioravanti@portlandoregon.gov](mailto:Kara.Fioravanti@portlandoregon.gov)

## **NOTICE OF A TYPE II DECISION ON A PROPOSAL IN YOUR NEIGHBORHOOD**

The Bureau of Development Services has approved a proposal in your neighborhood. The reasons for the decision are included in the version located on the BDS website <http://www.portlandonline.com/bds/index.cfm?c=46429>. Click on the District Coalition then scroll to the relevant Neighborhood, and case number. If you disagree with the decision, you can appeal. Information on how to do so is included at the end of this decision.

### **CASE FILE NUMBER: LU 12-200244 DZ**

*REVISIONS TO PREVIOUSLY APPROVED BUILDING (LU 11-160898 DZM)*

#### **GENERAL INFORMATION**

**Applicant:** OHSU, Owner  
3181 SW Sam Jackson Park Road / Portland, OR 97239

**Representative:** George Hager, Architect / SERA Architects  
338 NW 5<sup>th</sup> Avenue / Portland, OR 97209

Mark Williams, Contact for OUS/OHSU Collaborative Life Science Bldg  
Skourtes Tower / 515 Market Center Building  
MCB PO Box 751 / Portland, OR 97207

**Site Address:** 0650 SW MEADE AVENUE  
**Legal Description:** TL 200 12.42 ACRES, SECTION 10 1S 1E  
**Tax Account No.:** R991100630  
**State ID No.:** 1S1E10 00200  
**Quarter Section:** 3329, 3330  
**Neighborhood:** South Portland NA., contact Jim Davis at 503-248-9820.  
**District Coalition:** Southwest Neighborhoods Inc., contact Leonard Gard at 503-823-4592.  
**Plan District:** Central City - South Waterfront  
**Zoning:** CXd, g - Central Commercial with design and greenway overlays  
**Case Type:** DZ, Design Review  
**Procedure:** Type II, an administrative decision with appeal to the Design Commission.

#### **Proposal:**

The applicant seeks design review approval for revisions to a previously approved Type III Design Review (LU 11-160898 DZM), which allowed the construction of the OUS/OHSU Collaborative Life Sciences Building and Skourtes Tower. The design revisions now proposed are minor in scope occurring around the building and include the following: the guardrail changed from stainless steel cable mesh to glass, some windows were added to the north and south towers, some windows were removed from the north tower, a skylight was removed from the central atrium roof, the outdoor roof terrace at the east end of the central atrium was reconfigured, some of the entry doors are different, some canopies changed from metal and red

glass to silver metal, a louver was added to the ground level at the central atrium's west elevation, and accordion seismic joints were added between the central atrium and the towers.

**Relevant Approval Criteria:**

In order to be approved, this proposal must comply with the approval criteria of Title 33. The relevant criteria are the Central City Fundamental Design Guidelines and the South Waterfront Design Guidelines.

Zoning Code Section 33.700.080 states that Land Use Review applications are reviewed under the regulations in effect at the time the application was submitted, provided that the application is complete at the time of submittal, or complete within 180 days. This application was submitted on October 30, 2012 and determined to be complete on November 13, 2012.

## ANALYSIS

**Site and Vicinity:** OHSU acquired the future OSHU Schnitzer Campus (784,375 SF of land) through a generous gift by the Schnitzer family. This building will be the first project built on the OHSU Schnitzer Campus. Prior uses include the dismantling of ships. In the early 1980's an effort to clean up years of heavy industrial use was undertaken. This effort included involvement by the Department of Environmental Quality. The resulting document called the Consent Decree became the standard for which the cleanup effort would be measured. After visible debris was removed from the site, a temporary soil cap was installed after testing the soil for contaminants. This temporary soil cap included a geotextile fabric demarcation layer and a new twelve inch clean compacted gravel layer. Current uses of the site include a surface parking lot for OHSU, though the land is primarily vacant. Security around the site is provided by a chain link fence.

A master plan (PDC Final December 17, 2010- South Waterfront North District Conceptual Street Grading and Utility Master Plan) was completed showing block layout, street layout, transportation locations and utility infrastructure. This master plan formed the base of which the proposed building is taking shape. The lot planned for development under this application is bound by SW Moody to the west, SW Meade to the north, the campus pedestrian promenade to the east, and SW Porter to the south – though SW Moody is currently being reconstructed and raised as much as 14', and the other frontages do not yet exist. SW Moody, as part of its current reconstruction, is being realigned. Because of that realignment, SW Moody will be approximately 45' wide from curb to property line – the additional 30' of right-of-way is referred to as the SW Moody remnant because it cannot be built upon given the infrastructure below the surface and the 14' tall retaining wall for the raised street. The new Portland/ Milwaukie Light Rail Bridge starting on SW Porter will provide transportation across the Willamette River. A MAX and bus transit stop will be located south of this building. Streetcar stops will be located west of this building. SW Meade Street is a designated Enhanced Pedestrian Street.

**Zoning:** The Central Commercial Zone (CX) is intended to provide for commercial development within Portland's most urban and intense areas. A broad range of uses is allowed to reflect Portland's role as a commercial, cultural and governmental center. Development is intended to be very intense with high building coverage, large buildings, and buildings placed close together. Development is intended to be pedestrian-oriented with a strong emphasis on a safe and attractive streetscape.

The Design Overlay Zone [d] promotes the conservation, enhancement, and continued vitality of areas of the City with special scenic, architectural, or cultural value. This is achieved through the creation of design districts and applying the Design Overlay Zone as part of community planning projects, development of design guidelines for each district, and by requiring design review. In addition, design review ensures that certain types of infill development will be compatible with the neighborhood and enhance the area.

The Greenway Overlay Zones, designated as “g”, “i”, “n”, “q” or “r” are intended to protect, conserve, enhance, and maintain the natural, scenic, historical, economic, and recreational qualities of lands along Portland's rivers; establish criteria, standards, and procedures for the development of land, change of uses, and the intensification of uses within the greenway; increase public access to and along the Willamette River for the purpose of increasing recreational opportunities, providing emergency vehicle access, assisting in flood protection and control, providing connections to other transportation systems, and helping to create a pleasant, aesthetically pleasing urban environment; implement the City's Willamette Greenway responsibilities as required by ORS 390.310 to 390.368; and implement the water quality performance standards of Metro's Title 3.

- The River General “g” allows for uses and development which are consistent with the base zoning, which allow for public use and enjoyment of the waterfront, and which enhance the river's natural and scenic qualities.

**Land Use History:** City records indicate that prior land use reviews since 1994 include:

- **LU 12-117334 PR** approved, with conditions, a Central City Parking Review for an extension of the surface parking on this site. The Conditions of Approval are as follows:
  - A. The proposed surface parking lot on the Unit B/Schnitzer Lot shall close and the parking operations shall be removed by January 1, 2028.
  - B. To implement Condition B, OHSU is required to submit, no later than June 1, 2027, a surface parking lot closure/removal plan to the City Engineer for review and approval. This plan shall address physical closure of OHSU surface parking operations under Condition B of this decision and shall be completed in a manner acceptable to the City Engineer. This review will include oversight by Bureau of Development Services Design Review staff to ensure compliance with any possible applicable zoning code requirements.
- **LU 11-160898 DZM** was a Type III Design Review approval for the Collaborative Life Sciences Building/Skourtes Tower.
- **EA 11-143232 DA** was the Design Advice Request for the Collaborative Life Sciences Building/Skourtes Tower
- **EA 11-103984 DA** was withdrawn.
- **LU 10-151765 DZ, GW** was the Design Review and Greenway Review for the Willamette River Transit Bridge for the Portland-Milwaukie Light Rail extension.
- **LU 07-182840 DZ** was voided.
- **LU 05-181180 DZM** was the Design Review approval for the surface parking on this site.
- **LU 05-125472 PR** approved, with conditions, a Central City Parking Review for the surface parking on this site. The Conditions of Approval are as follows:
  - B. The proposed surface parking lot on the Unit B/Schnitzer Lot shall close and the parking operations shall be removed by January 1, 2013.
  - C. To implement Condition B, OHSU is required to submit, no later than June 1, 2012, a surface parking lot closure/removal plan to the City Engineer for review and approval. This plan shall address physical closure of OHSU surface parking operations under Condition B of this decision and shall be completed in a manner acceptable to the City Engineer. This review will include oversight by Bureau of Development Services Design Review staff to ensure compliance with any possible applicable zoning code requirements.
- **LUR 01-00403 GW, AD** was an approval of Greenway Review for construction of a stormwater outfall and decommissioning of two outfalls, and Adjustments to 33.272.030, Construction of (Public Recreation) Trails, and 33.440.230, Landscaping, to defer Greenway trail construction and riverbank landscaping until an area-wide development plan is formulated.
- **LUR 95-00611 GW** was withdrawn.
- Property line adjustments associated with this site include: **10-154241 and 10-158465.**
- Exempt Reviews for Department of Environmental Quality clean-up activity include: **10-190362, 10-120319 and 10-103126.**

**Agency Review:** A “Notice of Proposal in Your Neighborhood” was mailed on November 13, 2012. The following Bureaus have responded with no issues or concerns Bureau of Development Services, Life Safety; Water Bureau; Site Development; Bureau of Transportation Engineering; Fire Bureau.

- Bureau of Environmental Services, Exhibit E.1

“BES has no objection to the requested design review, provided that proposed changes to the outdoor roof terrace do not impact the ecoroof approved as part of building permit 11-140071-MG. Because the ecoroof is part of the required stormwater system for the building, any changes to it, along with supporting documentation and calculations, must be submitted and reviewed through a revision to the building permit.”

**Neighborhood Review:** A Notice of Proposal in Your Neighborhood was mailed on November 13, 2012. No written responses have been received from either the Neighborhood Association or notified property owners in response to the proposal.

## ZONING CODE APPROVAL CRITERIA

### Chapter 33.825 Design Review

#### Section 33.825.010 Purpose of Design Review

Design review ensures that development conserves and enhances the recognized special design values of a site or area. Design review is used to ensure the conservation, enhancement, and continued vitality of the identified scenic, architectural, and cultural values of each design district or area. Design review ensures that certain types of infill development will be compatible with the neighborhood and enhance the area. Design review is also used in certain cases to review public and private projects to ensure that they are of a high design quality.

#### Section 33.825.055 Design Review Approval Criteria

A design review application will be approved if the review body finds the applicant to have shown that the proposal complies with the design guidelines for the area.

**Findings:** The site is designated with a design (d) overlay zone, therefore the proposal requires Design Review approval. Because of the site’s location, the applicable design guidelines are the Central City Fundamental Design Guidelines and the South Waterfront Design Guidelines.

#### Central City Plan Design Goals

1. Encourage urban design excellence in the Central City;
2. Integrate urban design and preservation of our heritage into the development process;
3. Enhance the character of the Central City’s districts;
4. Promote the development of diversity and areas of special character within the Central City;
5. Establish an urban design relationship between the Central City’s districts and the Central City as a whole;
6. Provide for a pleasant, rich and diverse pedestrian experience for pedestrians;
7. Provide for the humanization of the Central City through promotion of the arts;
8. Assist in creating a 24-hour Central City which is safe, humane and prosperous;
9. Ensure that new development is at a human scale and that it relates to the scale and desired character of its setting and the Central City as a whole.

#### South Waterfront Design Goals

The South Waterfront Design Guidelines and the Greenway Design Guidelines for the South Waterfront supplement the Central City Fundamental Design Guidelines. These two sets of guidelines add layers of specificity to the fundamentals, addressing design issues unique to South Waterfront and its greenway.

The South Waterfront Design Guidelines apply to all development proposals in South Waterfront within the design overlay zone, identified on zoning maps with the lowercase letter

“d”. These guidelines primarily focus on the design characteristics of buildings in the area, including those along Macadam Avenue, at the western edge, to those facing the greenway and river.

The Greenway Design Guidelines for the South Waterfront apply to development within the greenway overlay zone, identified on zoning maps with a lowercase “g”. These design guidelines focus on the area roughly between the facades of buildings facing the river and the water’s edge.

### **South Waterfront Design Guidelines and Central City Fundamental Design Guidelines**

The Central City Fundamental Design and the South Waterfront Design Guidelines and the Greenway Design Guidelines for South Waterfront focus on four general categories. **(A) Portland Personality**, addresses design issues and elements that reinforce and enhance Portland’s character. **(B) Pedestrian Emphasis**, addresses design issues and elements that contribute to a successful pedestrian environment. **(C) Project Design**, addresses specific building characteristics and their relationships to the public environment. **(D) Special Areas**, provides design guidelines for the four special areas of the Central City.

*Staff has considered all guidelines and has addressed only those guidelines considered applicable to this project.*

- A8. Contribute to a Vibrant Streetscape.**
- B1. Reinforce and Enhance the Pedestrian System.**
- C2. Promote Quality and Permanence in Development.**
- C3. Respect Architectural Integrity.**
- C4. Complement the Context of Existing Buildings.**
- C5. Design for Coherency.**
- C11. Integrate Roofs and Use Rooftops.**

**Findings:** The integrity of the originally approved building still stands with the proposed minor changes around the facades. The ground level pedestrian amenities (canopies and prominent doorways) still remain with color and material changes. The roof is slightly changed, but the exuberance of the roof forms and retention of eco-roof area will maintain the design excellence. Roof screening is different, but results in the same goal of minimizing rooftop equipment. And, the guardrail has changed, but has a similar quality of the initial material and fulfills the initial goal of disappearing. The slight window changes do not impact the coherency or integrity of what was previously approved. Finally, the added louver at the west elevation is not directly adjacent to the street, nor an entrance. The applicable guidelines are met with the proposed revisions being incorporated into the building.

## **DEVELOPMENT STANDARDS**

Unless specifically required in the approval criteria listed above, this proposal does not have to meet the development standards in order to be approved during this review process. The plans submitted for a building or zoning permit must demonstrate that all development standards of Title 33 can be met, or have received an Adjustment or Modification via a land use review prior to the approval of a building or zoning permit.

## **CONCLUSIONS**

The approval criteria are met.

## **ADMINISTRATIVE DECISION**

Approval of design review for revisions to a previously approved Type III Design Review (LU 11-160898 DZM), which allowed the construction of the OUS/OHSU Collaborative Life Sciences

Building and Skourtes Tower. Approval per the approved plans, Exhibits C-1 through C-32, signed and dated January 30, 2013, subject to the following conditions:

- A. As part of the building permit application submittal, each of the 4 required site plans and any additional drawings must reflect the information and design approved by this land use review as indicated in Exhibits C.1-C.32. The sheets on which this information appears must be labeled, "Proposal and design as approved in Case File # LU 12-200244 DZ. No field changes allowed."

**Staff Planner: Kara Fioravanti**

**Decision rendered by:**  **on January 30, 2013**

By authority of the Director of the Bureau of Development Services

**Decision mailed: February 1, 2013**

**About this Decision.** This land use decision is **not a permit** for development. Permits may be required prior to any work. Contact the Development Services Center at 503-823-7310 for information about permits.

**Procedural Information.** The application for this land use review was submitted on October 30, 2012, and was determined to be complete on **November 13, 2012**.

*Zoning Code Section 33.700.080* states that Land Use Review applications are reviewed under the regulations in effect at the time the application was submitted, provided that the application is complete at the time of submittal, or complete within 180 days. Therefore this application was reviewed against the Zoning Code in effect on October 30, 2012.

*ORS 227.178* states the City must issue a final decision on Land Use Review applications within 120-days of the application being deemed complete. The 120-day review period may be waived or extended at the request of the applicant. In this case, the applicant did not waive or extend the 120-day review period. Unless further extended by the applicant, **the 120 days will expire on: March 13, 2013**.

**Some of the information contained in this report was provided by the applicant.**

As required by Section 33.800.060 of the Portland Zoning Code, the burden of proof is on the applicant to show that the approval criteria are met. The Bureau of Development Services has independently reviewed the information submitted by the applicant and has included this information only where the Bureau of Development Services has determined the information satisfactorily demonstrates compliance with the applicable approval criteria. This report is the decision of the Bureau of Development Services with input from other City and public agencies.

**Conditions of Approval.** If approved, this project may be subject to a number of specific conditions, listed above. Compliance with the applicable conditions of approval must be documented in all related permit applications. Plans and drawings submitted during the permitting process must illustrate how applicable conditions of approval are met. Any project elements that are specifically required by conditions of approval must be shown on the plans, and labeled as such.

These conditions of approval run with the land, unless modified by future land use reviews. As used in the conditions, the term "applicant" includes the applicant for this land use review, any person undertaking development pursuant to this land use review, the proprietor of the use or development approved by this land use review, and the current owner and future owners of the property subject to this land use review.

**Appealing this decision.** This decision may be appealed to the Design Commission, which will hold a public hearing. Appeals must be filed **by 4:30 PM on February 15, 2013** at 1900 SW

Fourth Ave. Appeals can be filed Tuesday through Friday on the first floor of the Development Services Center until 3 p.m. After 3 p.m. and Mondays, appeals must be submitted to the receptionist at the front desk on the fifth floor. **An appeal fee of \$250 will be charged.** The appeal fee will be refunded if the appellant prevails. There is no fee for ONI recognized organizations appealing a land use decision for property within the organization's boundaries. The vote to appeal must be in accordance with the organization's bylaws. Assistance in filing the appeal and information on fee waivers is available from BDS in the Development Services Center. Please see the appeal form for additional information.

The file and all evidence on this case are available for your review by appointment only. Please call the Request Line at our office, 1900 SW Fourth Avenue, Suite 5000, phone 503-823-7617, to schedule an appointment. I can provide some information over the phone. Copies of all information in the file can be obtained for a fee equal to the cost of services. Additional information about the City of Portland, city bureaus, and a digital copy of the Portland Zoning Code is available on the internet at [www.portlandonline.com](http://www.portlandonline.com).

**Attending the hearing.** If this decision is appealed, a hearing will be scheduled, and you will be notified of the date and time of the hearing. The decision of the Design Commission is final; any further appeal must be made to the Oregon Land Use Board of Appeals (LUBA) within 21 days of the date of mailing the decision, pursuant to ORS 197.620 and 197.830. Contact LUBA at 550 Capitol St. NE, Suite 235, Salem, Oregon 97301, or phone 1-503-373-1265 for further information.

Failure to raise an issue by the close of the record at or following the final hearing on this case, in person or by letter, may preclude an appeal to the Land Use Board of Appeals (LUBA) on that issue. Also, if you do not raise an issue with enough specificity to give the Design Commission an opportunity to respond to it, that also may preclude an appeal to LUBA on that issue.

**Recording the final decision.**

If this Land Use Review is approved the final decision must be recorded with the Multnomah County Recorder. A few days prior to the last day to appeal, the City will mail instructions to the applicant for recording the documents associated with their final land use decision.

- *Unless appealed,* The final decision may be recorded on or after **February 19, 2013.**
- A building or zoning permit will be issued only after the final decision is recorded.

The applicant, builder, or a representative may record the final decision as follows:

- By Mail: Send the two recording sheets (sent in separate mailing) and the final Land Use Review decision with a check made payable to the Multnomah County Recorder to: Multnomah County Recorder, P.O. Box 5007, Portland OR 97208. The recording fee is identified on the recording sheet. Please include a self-addressed, stamped envelope.
- In Person: Bring the two recording sheets (sent in separate mailing) and the final Land Use Review decision with a check made payable to the Multnomah County Recorder to the County Recorder's office located at 501 SE Hawthorne Boulevard, #158, Portland OR 97214. The recording fee is identified on the recording sheet.

For further information on recording, please call the County Recorder at 503-988-3034  
For further information on your recording documents please call the Bureau of Development Services Land Use Services Division at 503-823-0625.

**Expiration of this approval.** An approval expires three years from the date the final decision is rendered unless a building permit has been issued, or the approved activity has begun. Where a site has received approval for multiple developments, and a building permit is not issued for all of the approved development within three years of the date of the final decision, a new land use review will be required before a permit will be issued for the remaining development, subject to the Zoning Code in effect at that time.

**Applying for your permits.** A building permit, occupancy permit, or development permit may be required before carrying out an approved project. At the time they apply for a permit, permittees must demonstrate compliance with:

- All conditions imposed herein;
- All applicable development standards, unless specifically exempted as part of this land use review;
- All requirements of the building code; and
- All provisions of the Municipal Code of the City of Portland, and all other applicable ordinances, provisions and regulations of the City.

### **EXHIBITS**

NOT ATTACHED UNLESS INDICATED

- A. Applicant's Submittal
  1. List of proposed changes
  2. Originally approved drawings
- B. Zoning Map (attached)
- C. Plans/Drawings:
  1. **C.33R** – North West canopy color changed from red glass to silver 1/8” thick aluminum metal panel. (attached)
  2. **C.36R** – East terrace paver and eco-roof pattern revised as well as sloped roof changed from eco roof planters to PVC roofing and eco-roof planter at bottom of slope.
  3. **C.37R** – East terrace paver and eco-roof pattern revised as well as sloped roof changed from eco roof planters to PVC roofing and eco-roof planter at bottom of slope.
  4. **C.38R** – East terrace paver and eco-roof pattern revised as well as sloped roof changed from eco roof planters to PVC roofing and eco-roof planter at bottom of slope. Removed skylight in middle of roof and changed from metal roof to PVC roof.
  5. **C.40R** – East terrace paver and eco-roof pattern revised as well as sloped roof changed from eco roof planters to PVC roofing and eco-roof planter at bottom of slope. Removed skylight in middle of roof and changed from metal roof to PVC roof.
  6. **C.44R** – North Tower West Elevation ribbon windows changed to 4 punched windows. South Tower West, North and South elevation at roof open perforation pattern for chiller intake was removed and panel types were made to match adjacent typically random pattern of perforations.
  7. **C.45R** – South Tower West, North and South elevation at roof open perforation pattern for chiller intake was removed and panel types were made to match adjacent typically random pattern of perforations.
  8. **C.46R** – Revised perforated panel layout to add vision glass continuing the ribbon window pattern toward the west.
  9. **C.47R** – North Tower West Elevation ribbon windows changed to 4 punched windows. South Tower West, North and South elevation at roof open perforation pattern for chiller intake was removed and panel types were made to match adjacent typically random pattern of perforations. Revised perforated panel layout to add vision glass continuing the ribbon window pattern toward the west. Seismic joint covers changed from aluminum panel to accordion on walls only.
  10. **C.48R** – North Tower West Elevation ribbon windows changed to 4 punched windows. South Tower West, North and South elevation at roof open perforation pattern for chiller intake was removed and panel types were made to match adjacent typically random pattern of perforations. Revised perforated panel layout to add vision glass continuing the ribbon window pattern toward the west. Seismic joint cover changed from aluminum panel to accordion.
  11. **C.49R** – East terrace paver and eco-roof pattern revised as well as sloped roof changed from eco roof planters to PVC roofing and eco-roof planter at bottom of slope. Removed skylight in middle of roof and changed from metal roof to PVC roof.
  12. **C.50R** – East terrace paver and eco-roof pattern revised as well as sloped roof changed from eco roof planters to PVC roofing and eco-roof planter at bottom of slope. Revised



- perforated panel layout to add vision glass continuing the ribbon window pattern toward the west.
13. **C.51R** – North Tower West Elevation ribbon windows changed to 4 punched windows. South Tower West, North and South elevation at roof open perforation pattern for chiller intake was removed and panel types were made to match adjacent typically random pattern of perforations. Revised perforated panel layout to add vision glass continuing the ribbon window pattern toward the west.
  14. **C.52R** – North Tower West Elevation ribbon windows changed to 4 punched windows. South Tower West, North and South elevation at roof open perforation pattern for chiller intake was removed and panel types were made to match adjacent typically random pattern of perforations. Added louver on Level 1 West Elevation.
  15. **C.53R** – North West canopy changed from red to silver. Revised perforated panel layout to add vision glass continuing the ribbon window pattern toward the west.
  16. **C.54R** – East terrace paver and eco-roof pattern revised as well as sloped roof changed from eco roof planters to PVC roofing and eco-roof planter at bottom of slope. Seismic joint covers changed from aluminum panel to accordion on walls only.
  17. **C.55R** – North Tower Level 13 mechanical screen changed from perforated corrugated panels to solid corrugated panels. South Tower Level 2 Red metal panel strip at south façade slab edge changed from metal panel to red spandrel glass. South Tower West, North and South elevation at roof open perforation pattern for chiller intake was removed and panel types were made to match adjacent typically random pattern of perforations.
  18. **C.58R** – South Tower West, North and South elevation at roof open perforation pattern for chiller intake was removed and panel types were made to match adjacent typically random pattern of perforations.
  19. **C.61R** – Seismic joint covers changed from aluminum panel to accordion on walls only. Vestibule entry changed from red glass to red gypsum board at interior and exterior fins changed from red glass to red aluminum fins.
  20. **C.63R** – Vestibule entry changed from red glass to red gypsum board at interior and exterior fins changed from red glass to red aluminum fins. East terrace paver and eco-roof pattern revised as well as sloped roof changed from eco roof planters to PVC roofing and eco-roof planter at bottom of slope.
  21. **C.69R** – Added louver on Level 1 West Elevation.
  22. **C.72R** – North Tower West Elevation ribbon windows changed to 4 punched windows. South Tower West, North and South elevation at roof open perforation pattern for chiller intake was removed and panel types were made to match adjacent typically random pattern of perforations. Revised perforated panel layout to add vision glass continuing the ribbon window pattern toward the west.
  23. **C.73R** – North Tower West Elevation ribbon windows changed to 4 punched windows. Revised perforated panel layout to add vision glass continuing the ribbon window pattern toward the west. North West canopy changed from red glass to silver metal panel.
  24. **C.74R** – North tower middle canopy changed from red glass to red 1/8" thick aluminum plate panel.
  25. **C.76R** – North tower middle canopy changed from red glass to red 1/8" thick aluminum plate panel.
  26. **C.90R** – Vestibule entry changed from red glass to red gypsum board at interior and exterior fins changed from red glass to red aluminum fins.
  27. **C.99R** – East terrace guardrail changed from mesh to glass guardrail at back of parapet on east and south edges of East terrace Level 4.
  28. **C.113R** – Added louver on Level 1 West Elevation.
  29. **C.118R** – Seismic joint covers changed from aluminum panel to accordion on walls only. Vestibule entry changed from red glass to red gypsum board at interior and exterior fins changed from red glass to red aluminum fins.
  30. **C.121R** – Vestibule entry changed from red glass to red gypsum board at interior and exterior fins changed from red glass to red aluminum fins. Seismic joint covers changed from aluminum panel to accordion on walls only. Added louver on Level 1 West

Elevation. South Tower West, North and South elevation at roof open perforation pattern for chiller intake was removed and panel types were made to match adjacent typically random pattern of perforations.

31. **C.122R** – Vestibule entry changed from red glass to red gypsum board at interior and exterior fins changed from red glass to red aluminum fins.
32. **C.127** – Seismic Joint cover changed from aluminum panel to accordion - 3D image provided for visualization.

D. Notification information:

1. Mailing list
2. Mailed notice

E. Agency Responses:

1. Bureau of Environmental Services

F. Correspondence:

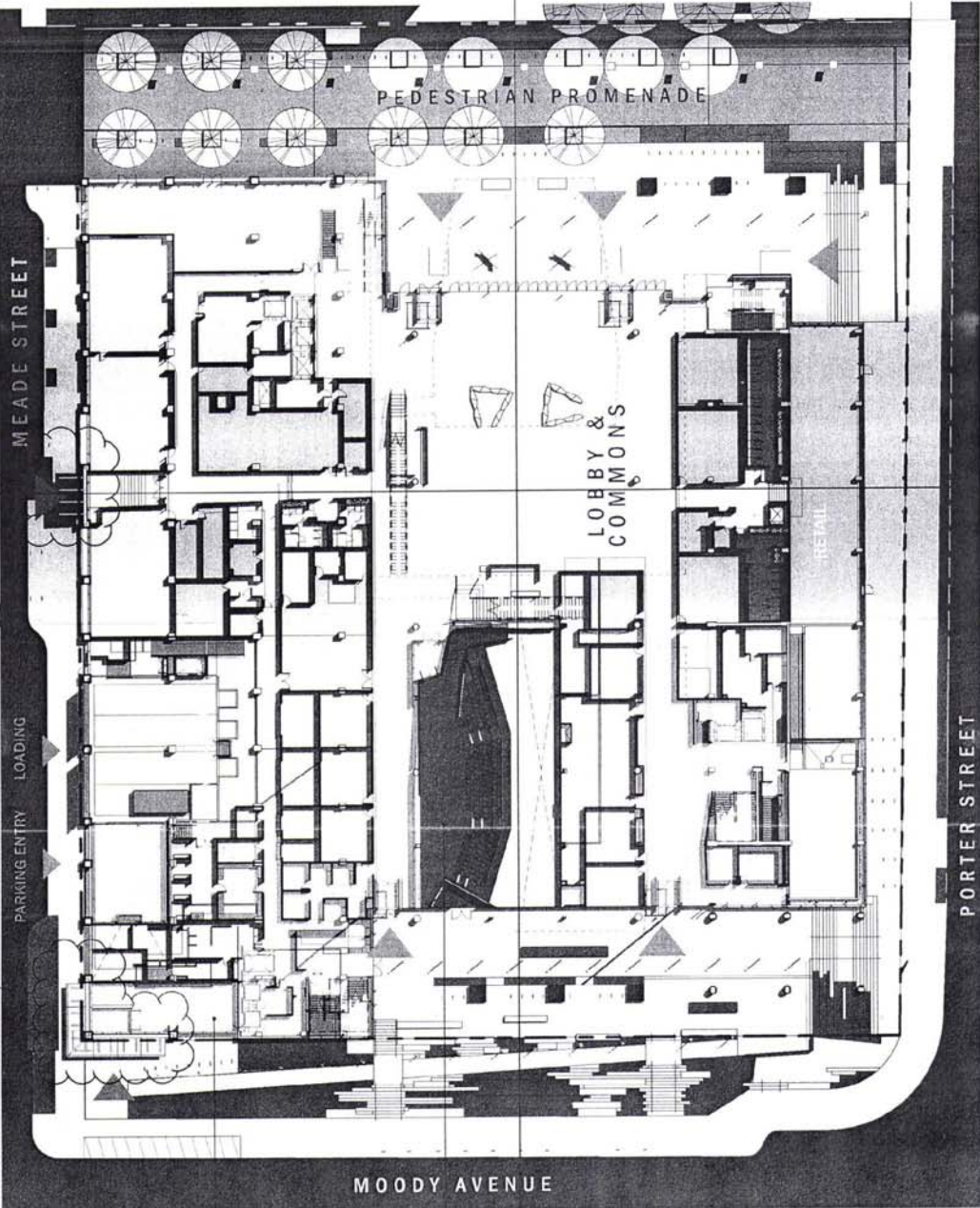
*None received*

G. Other:

1. Original LU Application
2. Site History Research

**The Bureau of Development Services is committed to providing equal access to information and hearings. Please notify us no less than five business days prior to the event if you need special accommodations. Call 503-823-7300 (TTY 503-823-6868).**





CANOPY CHANGED TO 1/8" ALUMINUM SILVER METAL PANEL  
 CANOPY CHANGED TO 1/8" ALUMINUM RED METAL PANEL

MEADE STREET

PARKING ENTRY LOADING

PEDESTRIAN PROMENADE

LOBBY & COMMONS

RETAIL

PORTER STREET

MOODY AVENUE

500-SEAT LECTURE HALL

DENTAL CLINIC LOBBY

**\*Approved\***  
 City of Portland  
 Bureau of Development Services  
 Planner \_\_\_\_\_  
 Date 1.30.13  
 \*This approval applies only to the reviews requested and is subject to all conditions of approval.  
 Additional zoning requirements apply.

- LEGEND**
- Community Life
  - Retail
  - Classroom
  - Class Support
  - Teaching
  - Laboratory
  - Building Support

12-200244

C-1



LEVEL 1 PLAN EXHIBIT C.33R



LU 11-160898 DZM, GW

OHSU / OHSU COLLABORATIVE LIFE SCIENCES BUILDING  
 DESIGN REVIEW DRAWINGS OCTOBER 23, 2012

