



City of Portland, Oregon
Bureau of Development Services
Land Use Services
 FROM CONCEPT TO CONSTRUCTION

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Date: Tuesday, October 22, 2013
To: Interested Person
From: Kate Marcello, Land Use Services
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NOTICE OF A TYPE II DECISION ON A PROPOSAL IN YOUR NEIGHBORHOOD

The Bureau of Development Services has approved a proposal in your neighborhood. The reasons for the decision are included in the version located on the BDS website, via this link: <http://www.portlandonline.com/bds/index.cfm?c=46429>. Click on the district coalition and then scroll to the relevant neighborhood and case number. If you disagree with the decision, you can appeal. Information on how to do so is included at the end of this decision.

CASE FILE NUMBER: LU 13-157554 DZM
“GO BY TRAM” Sign on West Façade of OHSU Center for Health & Healing
GENERAL INFORMATION

Applicants: Melissa Hayden, Security Signs Inc | 503-546-7114
 2424 SE Holgate Blvd | Portland OR 97202
 Joseph Platt, Security Signs Inc | 503-546-7109
 2424 SE Holgate Blvd | Portland OR 97202

Owner: Oregon Health & Science University
 3181 SW Sam Jackson Park Rd | Portland, OR 97239-3011

Site Address: 3303 SW Bond Avenue

Legal Description: LOT 8, WATERFRONT SOUTH **Tax Account No.:** R882450400
State ID No.: 1S1E10AC 00308 **Quarter Section:** 3330
Neighborhood: South Portland Neighborhood Assoc, Jim Gardner at 503-227-2096
Business District: South Portland Business Assoc, Kevin Countryman at 503-750-2984
District Coalition: Southwest Neighborhoods Inc.; Leonard Gard at 503-823-4592
Plan District: Central City Plan District; South Waterfront Subdistrict
Zoning: Central Commercial (CX) base zone; Design (d) overlay zone
Case Type: Design Review (DZ)
Procedure: Type II, which is an administrative decision with appeal to the Design Commission.

PROPOSAL: The applicant requests Design Review approval for a new projecting sign to be located on the west façade of the OHSU Center for Health & Healing. The subject building is located adjacent to the Portland Aerial Tram lower station. The upper station is located atop Marquam Hill, at the Kohler Pavilion on OHSU’s main campus.

The new sign is a double-sided projecting sign, with approximately 221.3 square feet of sign face area on each side. The sign will be attached to the building with two 8-inch metal square tube arms. The sign consists of three distinct parts:

- The OHSU “flame” logo is located at the top of the sign. It is made of fabricated aluminum with routed flat cut-out aluminum overlay with digitally printed graphic overlay. At the bottom of

the logo, there is a fabricated aluminum cabinet with a push-through decorative line and push-through lettering. Both the logo and cabinet are illuminated by a digitally programmable up-light on each side of the sign.

- The main part of the sign consists of a fabricated aluminum cabinet with a 3-inch square tube frame with expanded metal. On this background, on both sides of the sign, there are 4-inch horizontal metal square tube decorative elements and 3-inch-deep open channel letters with exposed red neon.
- The lower part of the sign consists of two 4-inch-deep fabricated cabinets. There is one cabinet on each side of the sign. Each cabinet has a routed aluminum face with push-through acrylic lettering.

The proposal includes three **Modifications** as follows:

1. According to 32.32.020.A (Table 2) in the Sign Code, the maximum size for an individual sign in the CX zone is 100 sq ft. The proposed sign will be about 221.3 sq ft in area. Therefore a Modification is required.
2. According to 32.32.020.A (Table 2) in the Sign Code and the length of the subject building's primary building wall, the building is allowed a maximum of 300 sq ft of sign area. Currently the building has about 199 sq ft of sign area. With the proposed sign, the building will have about 420.3 sq ft of sign area. Therefore a Modification is required.
3. According to 32.32.020.A (Table 3), a projecting sign in the CX zone is allowed to have a maximum of 30 sq ft of sign area projecting into the right-of-way. The proposed sign will have about 221.3 sq ft of sign area projecting into the sidewalk right-of-way on SW Moody Avenue. Therefore a Modification is required.

Because the proposal is for non-exempt exterior alterations at a site with design overlay zoning in the Central City Plan District, Design Review approval is required prior to the issuance of permits.

RELEVANT APPROVAL CRITERIA:

In order to be approved, this proposal must comply with the approval criteria of Title 33 (Portland Zoning Code). The relevant criteria are:

- The *Central City Fundamental Design Guidelines*;
- The *South Waterfront Design Guidelines*;
- 33.825.040.A-B: *Approval Criteria for Modification through Design Review*

The proposal must also comply with Title 32 (Sign Code).

ANALYSIS

- I. **Site and Vicinity:** The subject site is bounded by SW Moody Avenue to the west and SW Whitaker Street to the south. The site is bounded on the east by SW Bond Avenue and on the north by Gibbs Street, which are closed to automobile traffic at this location. SW Bond and Gibbs, which are open to pedestrians and bicyclists, appear as seamless parts of the surrounding plaza-type area. The downtown-bound Portland Streetcar alignment is located within this unique portion of Bond and Gibbs.

The lower station for the Portland Aerial Tram is located just north of the subject site. From the northern edge of the site, the tram can be seen overhead as it enters the station. The Gibbs Street Pedestrian Bridge, which provides access over Interstate 5 between South Waterfront and the Lair Hill neighborhood west of the Interstate, is located nearby. The stair and elevator shaft for the pedestrian bridge are located near the northwest corner of the subject building, just across SW Moody Avenue. The bicycle path that provides access between downtown and South Waterfront crosses over Moody Avenue

near the northwest corner of the subject building. The bicycle path has its own traffic signal at this location. There is also a pedestrian crosswalk at this location; it is parallel to the bicycle crossing. In addition, there is a southbound stop for the Portland Streetcar located within Moody Avenue, adjacent to these pedestrian and bicycle crossings. This convergence of transportation modes – aerial tram, bicycles, pedestrians, and streetcar – makes for a dynamic streetscape adjacent to the subject site.

The subject building was constructed in 2005. It is occupied by Oregon Health Sciences University's Center for Health and Healing. The building is 16 stories tall. The main entrances are located on the north and east facades.

According to Portland's Transportation System Plan (TSP), SW Moody Avenue at this location is classified as a Central City Transit/Pedestrian Street, City Bikeway, Community Main Street, District Collector Street, and Major Transit Priority Street. SW Whitaker Street at this location is classified as a Local Service Walkway and Local Service Bikeway. SW Bond Avenue, closed to automobiles at this location, is classified as a Central City Transit/Pedestrian Street, City Bikeway, Community Main Street, District Collector Street, and Major Transit Priority Street. SW Gibbs Street, closed to automobiles at this location, is classified as a City Walkway, City Bikeway, and Transit Access Street. The site is located in the North Macadam Pedestrian District.

II. Zoning:

Base Zone: The Central Commercial (CX) zone is intended to provide for commercial development within Portland's most urban and intense areas. A broad range of uses is allowed to reflect Portland's role as a commercial, cultural and governmental center. Development is intended to be very intense with high building coverage, large buildings, and buildings placed close together.

Overlay Zone: The Design (d) overlay zone promotes the conservation, enhancement, and continued vitality of areas of the City with special scenic, architectural, or cultural value. This is achieved through the creation of design districts and applying the Design overlay zone as part of community planning projects, development of design guidelines for each district, and by requiring design review. In addition, Design Review ensures that certain types of infill development will be compatible with the neighborhood and enhance the area.

III. Land Use History:

City records indicate the following prior land use reviews for the subject site, since 1995:

- 1. LUR 01-00192 DZ GW:** Design Review and Greenway Review for development associated with concurrent subdivision LUR 01-00178 SU (which was withdrawn).
- 2. LU 02-116252 SU GW:** Subdivision Review approval to divide the southern portion of this site into five lots, with new public streets and two open-space tracts. The review included Greenway approval for specific site preparation activities.
- 3. LU 02-124581 AD GW:** Adjustment and Greenway Review approval to allow general site grading and to defer installation of required greenway trail and landscaping.
- 4. LU 02-128184 SU GW:** Subdivision Review approval to divide the northern portion of this site into eight lots, with new public streets and an open-space tract. The review included Greenway approval for specific site preparation activities.
- 5. LU 03-117500 GW:** Greenway Review approval to allow improvements and temporary activities.
- 6. LU 03-171715 DZ GW:** Design Review and Greenway Review approval to allow excavation and construction activities in the South Waterfront Greenway Area.
- 7. LU 04-004682 DZM:** Design Review approval for 16-story building with Modification to Height standard.
- 8. LU 05-125472 PR:** Approval of Central City Parking Review for new parking for new building approved in LU 04-004682 DZM.

- 9. LU 05-181180 DZM:** Design Review approval for new parking lots associated with LU 05-125472 PR; approval for two Modifications to Parking Area Layouts standard.
- 10. LU 06-105942 DZ:** Design Review approval for two parapet signs, each approximately 18 feet wide x 4 feet tall.

IV. Public Notice: A *Notice of Proposal in Your Neighborhood* was mailed on September 24, 2013.

Neighborhood Review: Three written comments in response to the proposal have been received.

1. On September 26, 2013, Tom and Mary Tripp submitted an e-mail with concerns regarding the funding source for the proposed sign, the seeming lack of necessity for the sign, and the possibility that the sign could distract motorists. Regarding motorist distraction: “[The sign will] certainly distract drivers at a very dangerous spot for bikers and pedestrians since people violate the ped/bike lights all the time.” Exhibit F-1 contains additional details.

Staff Response:

- Brian Newman, OHSU director of Campus Planning, Development, and Real Estate, e-mailed Tom and Mary Tripp on October 9, 2013, with responses to their concerns. In part, his e-mail states that the sign is entirely funded by OHSU; the “design intent” of the sign is “to mimic the style and appearance” of the existing *Go By Train* and *Go By Streetcar* signs and “to celebrate the Aerial Tram as a critical transportation connection as important to Portland’s character as the streetcar and train;” and the sign “will be no more distracting than other illuminated signs all over town.” Mr. Newman’s e-mail is exhibited as Exhibit G-2.
 - Bob Haley of the Development Review Section of the Portland Bureau of Transportation (PBOT) has provided a response to the Tripps’ concern regarding motorist distraction. Please refer to the “Agency Response” section below. PBOT’s response is exhibited as Exhibit E-1.
2. On August 15, 2013, James H. Luke submitted an e-mail in objection to the proposal. The e-mail states in part: “Wouldn’t the sign money be better spent on scholarships for struggling students or maybe healthcare for someone who needs it?” and “...the reason for having sign ordinances is to keep signs to a reasonable size so that they are not offensive as people go about their everyday activities. This proposed sign is unnecessary.” The e-mail ends with: “The sign is not needed and should not be allowed.” Exhibit F-2 contains additional details.

Staff Response:

- The applicant’s funding priorities cannot be considered as part of the decision-making process for this Design Review. Staff can consider the applicable design guidelines only.
 - Staff’s findings under the *Modifications* section of this document explain the reasons that the applicant is allowed to modify three standards of 32.32.020.A in the Sign Code (maximum sign area for an individual sign, maximum sign area for an individual building, and maximum sign area of a projecting sign allowed to project into the right-of-way).
3. On September 16, 2013, James Gardner, chairperson of the Land Use Committee of the South Portland Neighborhood Association, submitted an e-mail containing a summary of main points discussed by the land use committee with Brian Newman of OHSU, at a meeting held on September 15, 2013. The e-mail also stated: “It is reasonable to infer that our committee supports the proposal.” Exhibit F-3 contains additional details.

Agency Review: The Development Review Section of the Portland Bureau of Transportation (PBOT) responded with (Exhibit E-1), as follows:

PBOT does not have any issues with the proposed sign. Its proposed location will not obstruct visibility of the sidewalk corridors, crosswalks, signal heads or traffic signage. If we understand the proposal correctly,

this is a static sign and should not distract drivers any more than other illuminated signs in town.

ZONING CODE APPROVAL CRITERIA

I. DESIGN REVIEW

Chapter 33.825 Design Review

Section 33.825.010 Purpose of Design Review

Design review ensures that development conserves and enhances the recognized special design values of a site or area. Design review is used to ensure the conservation, enhancement, and continued vitality of the identified scenic, architectural, and cultural values of each design district or area. Design review ensures that certain types of infill development will be compatible with the neighborhood and enhance the area. Design review is also used in certain cases to review public and private projects to ensure that they are of a high design quality.

Section 33.825.055 Design Review Approval Criteria

A design review application will be approved if the review body finds the applicant to have shown that the proposal complies with the design guidelines for the area.

Findings: The site is designated with design overlay zoning (d); therefore the proposal requires Design Review approval. Because of the site's location, the applicable design guidelines are the Central City Fundamental Design Guidelines and the South Waterfront Design Guidelines.

Staff has considered all guidelines and has addressed only those guidelines considered applicable to this project. The Central City and South Waterfront design guidelines are addressed concurrently.

A4. Use Unifying Elements

Integrate unifying elements and/or develop new features that help unify and connect individual buildings and different areas.

C4. Complement the Context of Existing Buildings.

Complement the context of existing buildings by using and adding to the local design vocabulary.

Findings for A4 and C4: South Waterfront, as a mixed-use area with residents, businesses, medical facilities, and public open spaces, is a relatively new neighborhood. Most buildings in South Waterfront have been constructed within the past ten years. Building elements, site furnishings, sidewalk elements such as streetlights and benches, landscaping, and signage all employ very high-quality, durable materials. These materials make for a pleasant pedestrian experience by imparting a sense of permanence and providing humanly scaled, visually interesting details. These characteristics, worthy of emulation, are reflected in the proposed sign. The sign is almost entirely comprised of metal, and the primary lettering located in the main portion of the sign consists of open channel letters with exposed neon. Push-through acrylic letters are used for the two smaller sets of lettering. A small upright on both sides of the sign provides illumination for the logo portion of the sign, calling attention to the sculptural qualities of the logo. The sign is located at the lower portion of the building, where its small-scale details can easily be viewed and enjoyed by passersby. The proposed sign is an appropriate addition to the South Waterfront neighborhood. *Therefore these guidelines are met.*

B2. Protect the Pedestrian.

Protect the pedestrian environment from vehicular movement. Develop integrated identification, sign, and sidewalk-oriented night-lighting systems that offer safety, interest, and diversity to the pedestrian. Incorporate building equipment, mechanical exhaust routing systems, and/or service areas in a manner that does not detract from the pedestrian environment.

B2-1. Incorporate Outdoor Lighting That Responds to Different Uses.

Place and direct exterior lighting to ensure that the ground level of the building and associated outdoor spaces are well lit at night. Integrate exterior lighting so that it does not detract from the uses of adjacent areas.

1. Providing attached directional lighting along building facades.
2. Developing light fixtures that offer direct and indirect light.

C2. Promote Quality and Permanence in Development.

Use design principles and building materials that promote quality and permanence.

C3. Respect Architectural Integrity.

Respect the original character of an existing building when modifying its exterior. Develop vertical and horizontal additions that are compatible with the existing building, to enhance the overall proposal's architectural integrity.

C5. Design for Coherency.

Integrate the different building and design elements including, but not limited to, construction materials, roofs, entrances, as well as window, door, sign, and lighting systems, to achieve a coherent composition.

C12. Integrate Exterior Lighting.

Integrate exterior lighting and its staging or structural components with the building's overall design concept. Use exterior lighting to highlight the building's architecture, being sensitive to its impacts on the skyline at night.

C13. Integrate Signs.

Integrate signs and their associated structural components with the building's overall design concept. Size, place, design, and light signs to not dominate the skyline. Signs should have only a minimal presence in the Portland skyline.

Findings for B2, B2-1, C2, C3, C5, C12, and C13: The new sign employs durable materials that impart a sense of permanence. The sign is almost entirely comprised of metal, with aluminum framing, decorative elements, cabinets, and attachment arms. The primary lettering located in the main portion of the sign consists of open channel letters with exposed neon. Push-through acrylic letters are used for the two smaller sets of lettering.

The main portion of the sign, which contains the primary set of signage lettering, is 1'-7" thick. Decorative horizontal metal bars on the main portion of the sign project slightly beyond this 1'-7" thickness, along with the lower cabinet on the sign (where the building identification lettering is located). This thickness is appropriate for the sign's overall height and width – the sign appears neither excessively thin nor overly bulky – resulting in well-balanced massing.

The height and width of the sign are also appropriate. The sign is large enough to act as a prominent, visually interesting element of the streetscape, but it is not so large that it obscures the building or streetscape or detracts from the pedestrian orientation of the area.

Indirect lighting, in the form of one small fixture on each side, is used to highlight the logo portion of the sign. The fixtures are light-emitting diodes (LEDs) with compact housing in an aluminum finish. Two additional forms of lighting are incorporated into the sign: Open channel letters in the main portion of the sign have exposed neon that outlines the shape of each letter, and two small metal cabinets – one at the upper portion of the sign and one at the lower portion – have push-through acrylic letters illuminated internally by LEDs. All three of these lighting methods allow the sign to be seen and enjoyed during nighttime hours, but the lighting is modest enough not to interfere with views of the neighborhood or skyline.

The new sign's location does not diminish the architectural integrity of the building. The sign is located at the lower portion of the building that projects northward. This portion of the building is a clearly delineated podium-type mass with a distinctly different storefront condition at the ground floor and large projecting window bay at the mid-section. This portion of the building, with its differentiation from the main 16-story building mass, is an

appropriate location for the sign. At this location, the sign does not obfuscate the architecture – such as the window and spandrel patterning, expression of materials, or delineation of floor levels – of the primary building mass.

The sign also does not diminish the design coherency of the podium-type building mass. The sign will be placed on the building wall in an area of unitized granite that is free from architectural ornamentation. The sign will be located far enough above the ground-floor storefront that it will not create visual clutter. Lastly, the upper edge of the sign (the logo portion) will be aligned with the upper edge of the large projecting window bay on the north façade.

The new sign is integrated with the subject building, incorporates appropriate levels of lighting, utilizes high-quality materials, and allows the building to remain a coherent architectural composition.

Therefore these guidelines are met.

C7. Design Corners that Build Active Intersections.

Use design elements including, but not limited to, varying building heights, changes in façade plane, large windows, awnings, canopies, marquees, signs, and pedestrian entrances to highlight building corners. Locate flexible sidewalk-level retail opportunities at building corners. Locate stairs, elevators, and other upper floor building access points toward the middle of the block.

Findings for C7: The new sign will be installed near the northwest corner of the building. Two streets do not intersect at this location; however, there is a traffic signal on SW Moody Avenue to facilitate the crossing of pedestrians and bicyclists. This is a major crossing point due to the immediately adjacent aerial tram station; the main entrance to the Center for Health & Healing; the large bicycle parking area; the plaza that seamlessly connects the tram station, building entrance and nearby northbound streetcar stop; the South Waterfront access point for the Gibbs Street Pedestrian Bridge; bicycle lanes; and the southbound streetcar stop located between the automobile travel lanes on Moody. Thus for all intents and purposes, the area adjacent to the northwest corner of the building comprises an “intersection.” In fact, it is one of the most unique intersections in the entire City of Portland, because of its convergence of transportation modes: pedestrians, bicycles, streetcar, aerial tram, and automobiles.

The new sign will contribute to the existing vibrancy of this area by highlighting the northwest corner of the building and emphasizing the presence of the aerial tram overhead and the tram station to the east. The upper edge of the sign will be aligned with the upper edge of the large projecting window bay on the north facade, adding delineation to this lower portion of the building from the floors above. This will further emphasize the building corner, establishing it as a visually integral part of this bustling “intersection.”

The Findings for Modification #1 contain information detailing how the size of the sign enables it to better meet this particular design guideline.

Therefore this guideline is met.

C10. Integrate Encroachments.

Size and place encroachments in the public right-of-way to visually and physically enhance the pedestrian environment. Locate permitted skybridges toward the middle of the block, and where they will be physically unobtrusive. Design skybridges to be visually level and transparent.

Findings for C10: The new sign constitutes a minor encroachment over the sidewalk right-of-way on the east side of SW Moody Avenue. The sign projects about 2'-4" over

the sidewalk. Its lower edge is located about 16'-4" above the sidewalk. The sign does not obstruct the visibility of sidewalk corridors, crosswalks, traffic signals, or traffic-related signage.

The sign enhances the pedestrian environment by incorporating visually interesting material and lighting details and emphasizing the presence of the aerial tram overhead and the tram station to the east. In addition, the sign acts as a wayfinding and identification tool in the burgeoning South Waterfront campus of OHSU. Currently, building identification signage is comprised of modestly sized metal letters located at two entrances: one on the north façade facing the plaza-type area, and one on the south façade facing SW Whitaker Street. With the new sign, the building will now have identification on SW Moody Avenue, which is perhaps the most prominent corridor into and through the South Waterfront neighborhood. This is appropriate, considering that the primary pedestrian path leading into the area from downtown and from the more southerly reaches of the neighborhood is located on Moody, as well as the streetcar.

Therefore this guideline is met.

II. MODIFICATIONS

33.825.040 Modifications That Will Better Meet Design Review Requirements:

The review body may consider modification of site-related development standards, including the sign standards of Chapters 32.32 and 32.34 of the Sign Code, as part of the design review process. These modifications are done as part of design review and are not required to go through the adjustment process. Adjustments to use-related development standards (such as floor area ratios, intensity of use, size of the use, number of units, or concentration of uses) are required to go through the adjustment process. Modifications that are denied through design review may be requested as an adjustment through the adjustment process. The review body will approve requested modifications if it finds that the applicant has shown that the following approval criteria are met:

A. Better meets design guidelines.

The resulting development will better meet the applicable design guidelines; and

B. Purpose of the standard.

On balance, the proposal will be consistent with the purpose of the standard for which a modification is requested.

MODIFICATION #1:

Per 32.32.020.A of Portland's Sign Code, the size limit for an individual sign on a building in the Central Commercial (CX) zone is 100 sq ft. The proposed sign will be about 221.3 square feet in area. Therefore a Modification is needed.

The purposes of **32.32.020.A** are stated in the Sign Code as follows:

- A. *To ensure that signs and awnings are designed, constructed, installed, and maintained according to minimum standards to safeguard life, health, property, and public welfare;*
- B. *To allow and promote positive conditions for sign communication while at the same time avoiding nuisances to nearby properties;*
- C. *To reflect and support the desired character and development patterns of the various zones, overlay zones, and plan districts and promote an attractive environment;*
- D. *To allow for adequate and effective signs in commercial and industrial zones while preventing signs from dominating the appearance of an area; and*
- E. *To ensure that the constitutionally protected right of free speech is protected.*

Findings for Criterion A: Allowing the new sign to be larger than 100 square feet in area allows the proposal to better meet Central City Fundamental Design Guideline *C7: Design Corners that*

Build Active Intersections. The new sign will be located near the northwest corner of the building, which is located at the intersection of SW Moody Avenue with a pedestrian crosswalk, bicycle crosswalk, and streetcar tracks. This intersection is one of the most unique, dynamic intersections in the entire City of Portland, because of its convergence of transportation modes: pedestrians, bicycles, streetcar, aerial tram, and automobiles. The findings for Guideline C7 in the preceding section of this document describe the unique intersection condition of this area.

The new sign will contribute to the existing vibrancy of this unique intersection by highlighting the northwest corner of the building. This building corner is integral to the sense of urban enclosure at this intersection; the new sign reinforces this role. Additionally, its location adjacent to the large projecting window bay on the north façade adds delineation to the lower podium-type portion of the building, further establishing it as a visually integral part of this bustling intersection. If the sign were 100 square feet or less to comply with the maximum sign area standard of 32.32.020.A, it would be unable to fulfill the roles described here. Its size is an indispensable part of its success in meeting Guideline C7.

Therefore this criterion is met.

Findings for Criterion B: The purposes of 32.32.020.A are met, as follows:

- The new sign does not endanger life, health, property, or public welfare. The sign will be engineered to comply with life safety requirements, and the sign will not obstruct the visibility of sidewalk corridors, crosswalks, signal heads, or traffic signage.
- The new sign allows positive conditions for sign communication by virtue of its location at the lower portion of the building, close to the building wall from which it projects. In this location, the sign is visible within the pedestrian environment but does not obstruct views of other signs on the building or signs on nearby buildings. It will not be a nuisance to nearby properties nor will it overpower the appearance of the area.
- By virtue of its location about 16'-4" above the sidewalk, where it can be seen and enjoyed by passersby, the sign supports the pedestrian-oriented character of the Central City Plan District and the South Waterfront Subdistrict.
- The new sign does not affect the constitutionally protected right of free speech.

Therefore this criterion is met.

MODIFICATIONS #2 AND #3:

Modification #2: Per 32.32.020.A of Portland's Sign Code and the length of the building's primary building wall, the building is allowed a maximum of 300 square feet of sign area. Currently the building has about 199 square feet of sign area. With the proposed sign, the building will have about 420.3 square feet of sign area. Therefore a Modification is needed.

Modification #3: Per 32.32.020.A of Portland's Sign Code, a projecting sign on a building in the CX zone is allowed to have a maximum of 30 square feet of sign area projecting into the right-of-way. The proposed sign will have about 420.3 square feet of sign area projecting into the sidewalk right-of-way on SW Moody Avenue. Therefore a Modification is needed.

The purposes of **32.32.020.A** are stated in the preceding section entitled Modification #1.

Findings for Criterion A: Allowing the subject building to exceed its total maximum sign area and allowing the sign to have more than 30 square feet of sign area projecting into the right-of-way enable the proposal to better meet Central City Fundamental Design Guideline C10:

Integrate Encroachments. This guideline states: "Size and place encroachments in the public right-of-way to visually and physically enhance the pedestrian environment." At the size proposed, the sign enhances the pedestrian environment by acting as a wayfinding and identification tool. Currently, there are four building identification signs: two at the parapet – which cannot serve pedestrians due to their location at the top of the building – and two at the ground floor. The two ground-floor signs are comprised of modestly sized sets of metal letters: one at the north façade entry and one at the south façade entry. The primary purpose of these

two signs is to enable building patrons to locate the main entry doors. With the new sign, the building will now have prominent identification appropriately located on SW Moody Avenue, which is perhaps the most heavily traveled corridor into and through the South Waterfront neighborhood. The primary pedestrian path leading into the area from downtown and from the more southerly reaches of the neighborhood runs parallel to Moody Avenue. The streetcar is also located on Moody.

Additionally, the pedestrian environment is visually enhanced, per Guideline C10, by the sign's variety of design elements – the red neon outlining the primary set of lettering, the decorative horizontal metal bars separating the words of the primary lettering, the push-through acrylic letters at the upper and lower sign portions, and the elongated metal mesh background element located at the main portion of the sign. These details come together to render a bold presence within the streetscape, celebrating both the aerial tram and the building itself. This would not be possible if the building were not allowed to exceed its total allowed amount of sign area, nor would it be possible if the sign projected only 30 square feet over the sidewalk.

Therefore this criterion is met.

Findings for Criterion B:

- The existing signage and proposed sign do not endanger life, health, property, or public welfare. The new sign will be engineered to comply with life safety requirements, and the sign will not obstruct the visibility of sidewalk corridors, crosswalks, signal heads, or traffic signage.
- The new sign allows positive conditions for sign communication by virtue of its location at the lower portion of the building, close to the building wall from which it projects. In this location, the sign is visible within the pedestrian environment but does not obstruct views of other signs on the building or signs on nearby buildings. It will not be a nuisance to nearby properties nor will it overpower the appearance of the area.
- By virtue of its location about 16'-4" above the sidewalk, where it can be seen and enjoyed by passersby, the sign supports the pedestrian-oriented character of the Central City Plan District and the South Waterfront Subdistrict.
- The new sign does not affect the constitutionally protected right of free speech.

Therefore this criterion is met.

DEVELOPMENT STANDARDS

Unless specifically required in the approval criteria listed above, this proposal does not have to meet the development standards in order to be approved during this review process. The plans submitted for a building or zoning permit must demonstrate that all development standards of Title 33 can be met, or have received an Adjustment or Modification via a land use review prior to the approval of a building or zoning permit.

CONCLUSIONS

The proposed sign employs durable materials, is located and sized appropriately for the subject building and the surrounding urban context, and allows the building to remain an architecturally coherent composition. The proposal meets the applicable approval criteria and therefore warrants approval.

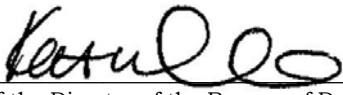
ADMINISTRATIVE DECISION

Approval of an approximately 221.3-square-foot projecting sign on the west façade of the OHSU Center for Health and Healing, located in the South Waterfront Subdistrict of the Central City Plan District.

Approval per the approved plans and drawings, Exhibits C-1 through C-7, signed and dated October 18, 2013, subject to the following condition:

- A. As part of the building permit application submittal, each of the four required site plans and any additional drawings must reflect the information and design approved by this land use review as indicated in Exhibits C-1 through C-7. The sheets on which this information appears must be labeled, "Proposal and design as approved in Case File # LU 13-157554 DZM. No field changes allowed."

Staff Planner: Kate Marcello

Decision rendered by:  **on October 18, 2013**
By authority of the Director of the Bureau of Development Services

Decision mailed: October 22, 2013

About this Decision. This land use decision is **not a permit** for development. Permits may be required prior to any work. Contact the Development Services Center at 503-823-7310 for information about permits.

Procedural Information. The application for this land use review was submitted on May 21, 2013, and was determined to be complete on September 19, 2013.

Zoning Code Section 33.700.080 states that Land Use Review applications are reviewed under the regulations in effect at the time the application was submitted, provided that the application is complete at the time of submittal, or complete within 180 days. Therefore this application was reviewed against the Zoning Code in effect on May 21, 2013.

ORS 227.178 states the City must issue a final decision on Land Use Review applications within 120-days of the application being deemed complete. The 120-day review period may be waived or extended at the request of the applicant. In this case, the applicant did not waive or extend the 120-day review period. Unless extended by the applicant, **the 120 days will expire on January 17, 2014.**

Some of the information contained in this report was provided by the applicant.

As required by Section 33.800.060 of the Portland Zoning Code, the burden of proof is on the applicant to show that the approval criteria are met. The Bureau of Development Services has independently reviewed the information submitted by the applicant and has included this information only where the Bureau of Development Services has determined the information satisfactorily demonstrates compliance with the applicable approval criteria. This report is the decision of the Bureau of Development Services with input from other City and public agencies.

Conditions of Approval. If approved, this project may be subject to a number of specific conditions, listed above. Compliance with the applicable conditions of approval must be documented in all related permit applications. Plans and drawings submitted during the permitting process must illustrate how applicable conditions of approval are met. Any project elements that are specifically required by conditions of approval must be shown on the plans, and labeled as such.

These conditions of approval run with the land, unless modified by future land use reviews. As used in the conditions, the term "applicant" includes the applicant for this land use review, any person undertaking development pursuant to this land use review, the proprietor of the use or development approved by this land use review, and the current owner and future owners of the property subject to this land use review.

Appealing this decision. This decision may be appealed to the Design Commission, which will hold a public hearing. Appeals must be filed **by 4:30 PM on November 5, 2013** at 1900 SW Fourth Ave. Appeals can be filed Tuesday through Friday on the first floor of the Development Services Center until 3 p.m. After 3 p.m. and Mondays, appeals must be submitted to the receptionist at the front desk on the fifth floor. **An appeal fee of \$250 will be charged.** The

appeal fee will be refunded if the appellant prevails. There is no fee for ONI recognized organizations appealing a land use decision for property within the organization's boundaries. The vote to appeal must be in accordance with the organization's bylaws. Assistance in filing the appeal and information on fee waivers is available from BDS in the Development Services Center. Please see the appeal form for additional information.

The file and all evidence on this case are available for your review by appointment only. Please call the Request Line at our office, 1900 SW Fourth Avenue, Suite 5000, phone 503-823-7617, to schedule an appointment. I can provide some information over the phone. Copies of all information in the file can be obtained for a fee equal to the cost of services. Additional information about the City of Portland, city bureaus, and a digital copy of the Portland Zoning Code is available on the internet at www.portlandonline.com.

Attending the hearing. If this decision is appealed, a hearing will be scheduled, and you will be notified of the date and time of the hearing. The decision of the Design Commission is final; any further appeal must be made to the Oregon Land Use Board of Appeals (LUBA) within 21 days of the date of mailing the decision, pursuant to ORS 197.620 and 197.830. Contact LUBA at 775 Summer St NE, Suite 330, Salem, OR 97301-1283, or phone 1-503-373-1265 for further information.

Failure to raise an issue by the close of the record at or following the final hearing on this case, in person or by letter, may preclude an appeal to the Land Use Board of Appeals (LUBA) on that issue. Also, if you do not raise an issue with enough specificity to give the Design Commission an opportunity to respond to it, that also may preclude an appeal to LUBA on that issue.

Recording the final decision.

If this Land Use Review is approved the final decision must be recorded with the Multnomah County Recorder. A few days prior to the last day to appeal, the City will mail instructions to the applicant for recording the documents associated with their final land use decision.

- *Unless appealed*, The final decision may be recorded on or after **November 6, 2013**.
- A building or zoning permit will be issued only after the final decision is recorded.

The applicant, builder, or a representative may record the final decision as follows:

- **By Mail:** Send the two recording sheets (sent in separate mailing) and the final Land Use Review decision with a check made payable to the Multnomah County Recorder to: Multnomah County Recorder, P.O. Box 5007, Portland OR 97208. The recording fee is identified on the recording sheet. Please include a self-addressed, stamped envelope.
- **In Person:** Bring the two recording sheets (sent in separate mailing) and the final Land Use Review decision with a check made payable to the Multnomah County Recorder to the County Recorder's office located at 501 SE Hawthorne Boulevard, #158, Portland OR 97214. The recording fee is identified on the recording sheet.

For further information on recording, please call the County Recorder at 503-988-3034
For further information on your recording documents please call the Bureau of Development Services Land Use Services Division at 503-823-0625.

Expiration of this approval. An approval expires three years from the date the final decision is rendered unless a building permit has been issued, or the approved activity has begun.

Where a site has received approval for multiple developments, and a building permit is not issued for all of the approved development within three years of the date of the final decision, a new land use review will be required before a permit will be issued for the remaining development, subject to the Zoning Code in effect at that time.

Applying for your permits. A building permit, occupancy permit, or development permit may be required before carrying out an approved project. At the time they apply for a permit, permittees must demonstrate compliance with:

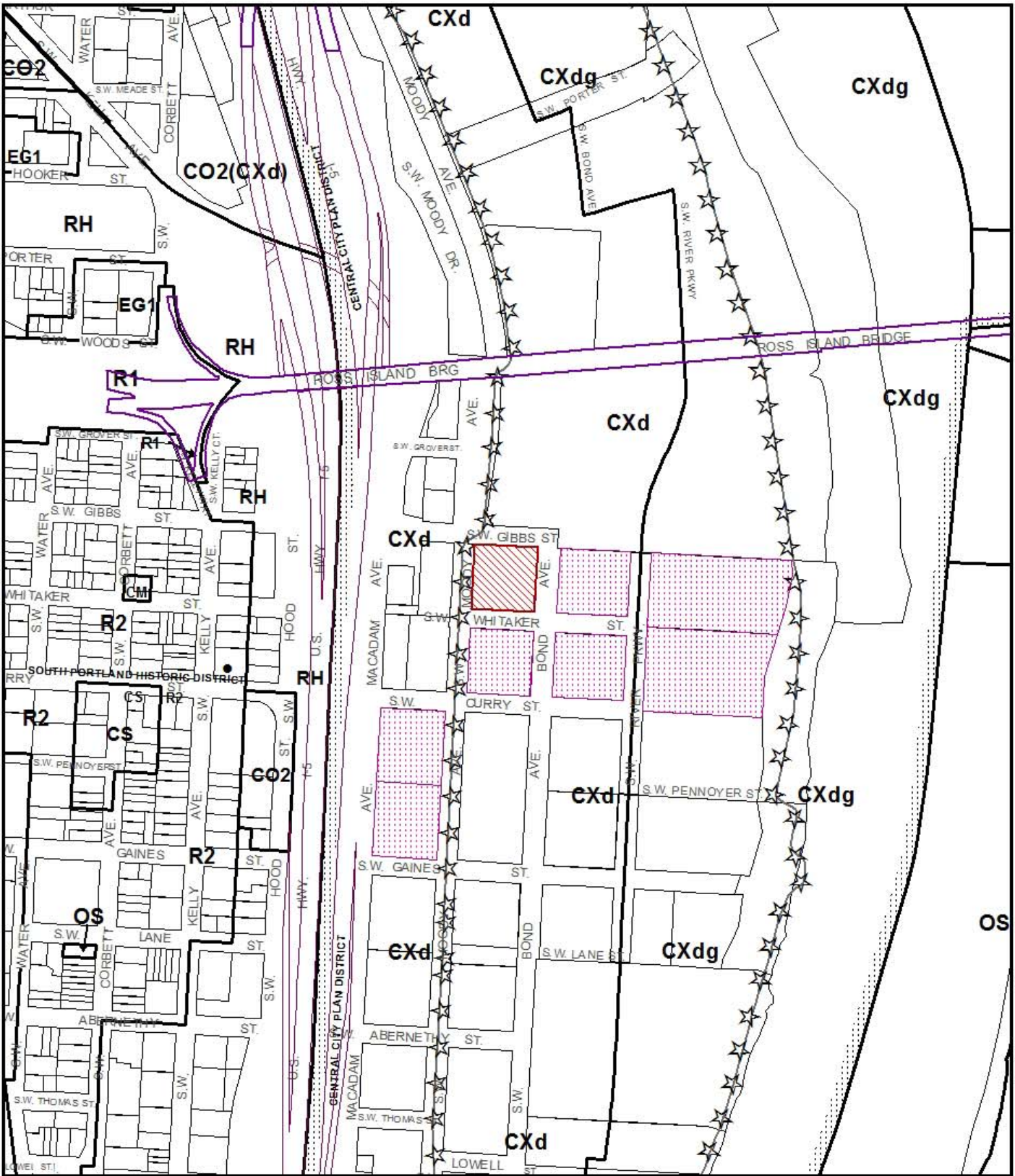
- All conditions imposed herein;
- All applicable development standards, unless specifically exempted as part of this land use review;
- All requirements of the building code; and
- All provisions of the Municipal Code of the City of Portland, and all other applicable ordinances, provisions and regulations of the City.

EXHIBITS

NOT ATTACHED UNLESS INDICATED

- A. Applicant's Statement/Narrative: None provided.
- B. Zoning Map (attached)
- C. Plans and Drawings:
 - 1. Site Plan (attached)
 - 2. North and West Building Elevation Drawings (attached)
 - 3. Enlarged North and West Building Elevation Drawings
 - 4. Photographic Simulation – Day and Night Views
 - 5. Enlarged Elevation Drawing and End View of Sign (attached)
 - 6. Enlarged Elevation Drawing of Sign, with Dimensions Indicated
 - 7. Illumination Specifications
- D. Notification information:
 - 1. Mailing list
 - 2. Mailed notice
- E. Agency Response:
 - 1. Development Review Section of Portland Bureau of Transportation
- F. Correspondence:
 - 1. Tom and Mary Tripp; received 26 September 2013; concerns regarding funding source for sign, seeming lack of necessity for sign, and possible motorist distraction
 - 2. James H. Luke; received 15 October 2013; in objection to the proposal
 - 3. James Gardner, chairperson of Land Use Committee of South Portland Neighborhood Association; received 16 October 2013; summary of discussion at land use committee meeting; no objections to proposal
- G. Other
 - 1. Original Land Use Review Application
 - 2. E-mail from Brian Newman, OHSU to Tom and Mary Tripp, in response to their concerns regarding funding source of sign and possible motorist distraction
 - 3. Site Visit Photographs
 - 4. Letter of Completeness, from Chris Beanes (former planner assigned to case) to Applicant, 5 June 2013

The Bureau of Development Services is committed to providing equal access to information and hearings. Please notify us no less than five business days prior to the event if you need special accommodations. Call 503-823-7300 (TTY 503-823-6868).



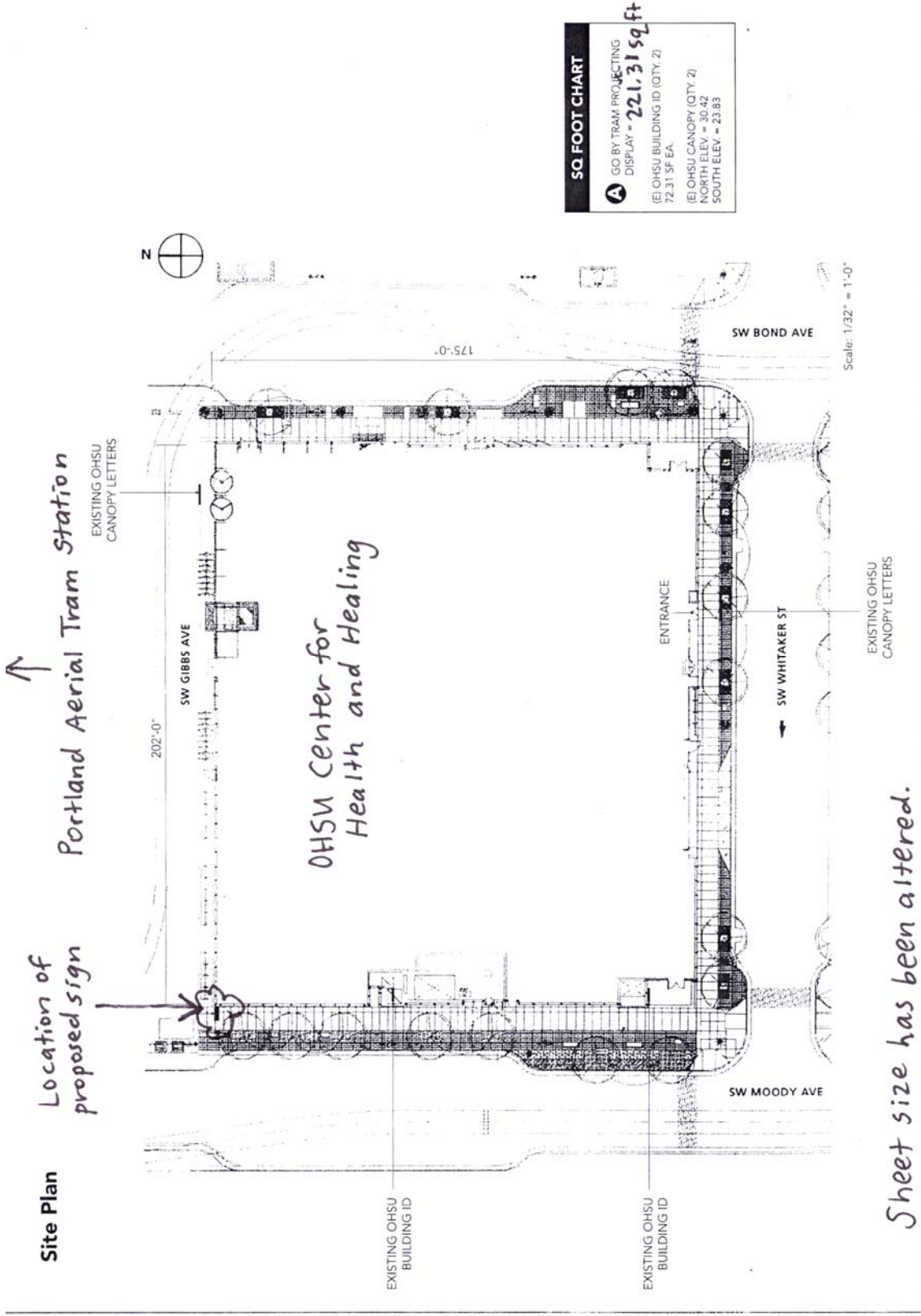
ZONING

-  Site
-  Also Owned



This site lies within the:
CENTRAL CITY PLAN DISTRICT
SOUTH WATERFRONT

File No.	LU 13-157554 DZM
1/4 Section	3330
Scale	1 inch = 400 feet
State_Id	1S1E10AC 308
Exhibit	B (May 23, 2013)



Site Plan
 Location of proposed sign

Portland Aerial Tram Station
 EXISTING OHSU CANOPY LETTERS

OHSU Center for Health and Healing

Sheet size has been altered.
 3303 SW Bond Ave

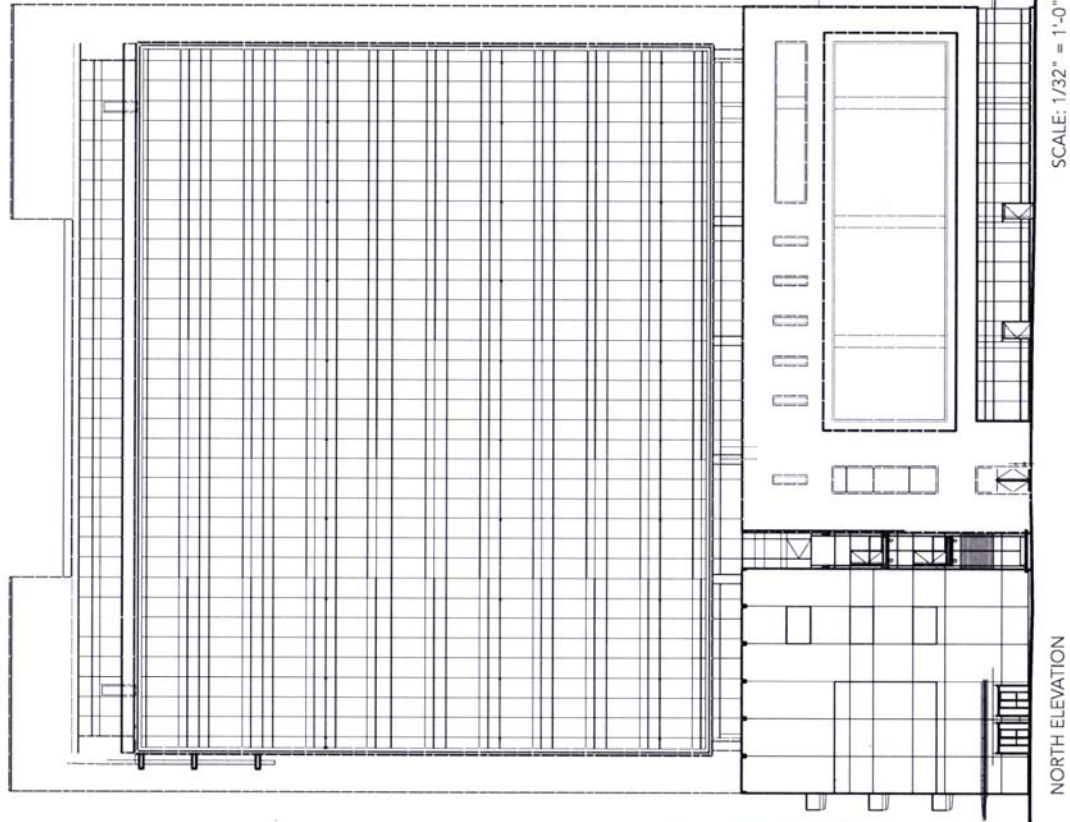
SQ. FOOT CHART
 GO BY TRAM PROJECTING DISPLAY - 221,315 sq. ft.
 (E) OHSU BUILDING ID (QTY: 2) 72.31 SF EA
 (E) OHSU CANOPY (QTY: 2) NORTH ELEV = 30.42 SOUTH ELEV = 23.83

Approved
 City of Portland - Bureau of Development Services
 Planner Kastel Date October 18, 2013
 * This approval applies only to the reviews requested and is subject to all conditions of approval. Additional zoning requirements may apply.

Case number: LU 13-157554 DZM

Exhibit C-1

Exterior SA | Projecting Identification - Full Building Elevation

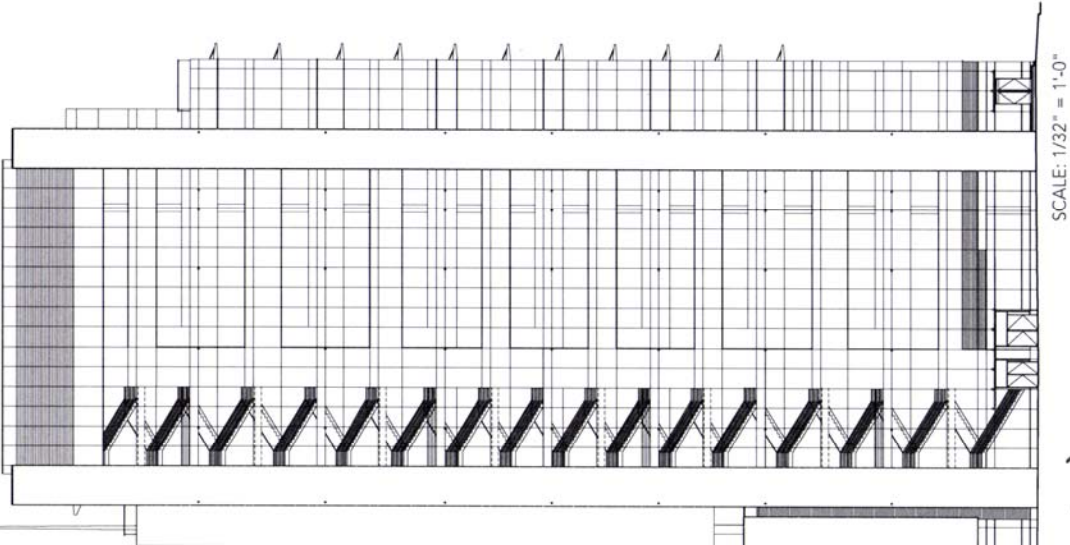


NORTH ELEVATION

SCALE: 1/32" = 1'-0"

Exhibit C-2

Case number:
LU 13-157554 DZM



WEST ELEVATION

SCALE: 1/32" = 1'-0"

* Approved*
City of Portland - Bureau of Development Services
Planner *[Signature]*
Date *October 18, 2013*
* This approval applies only to the reviews requested and is subject to all conditions of approval. Additional zoning requirements may apply.

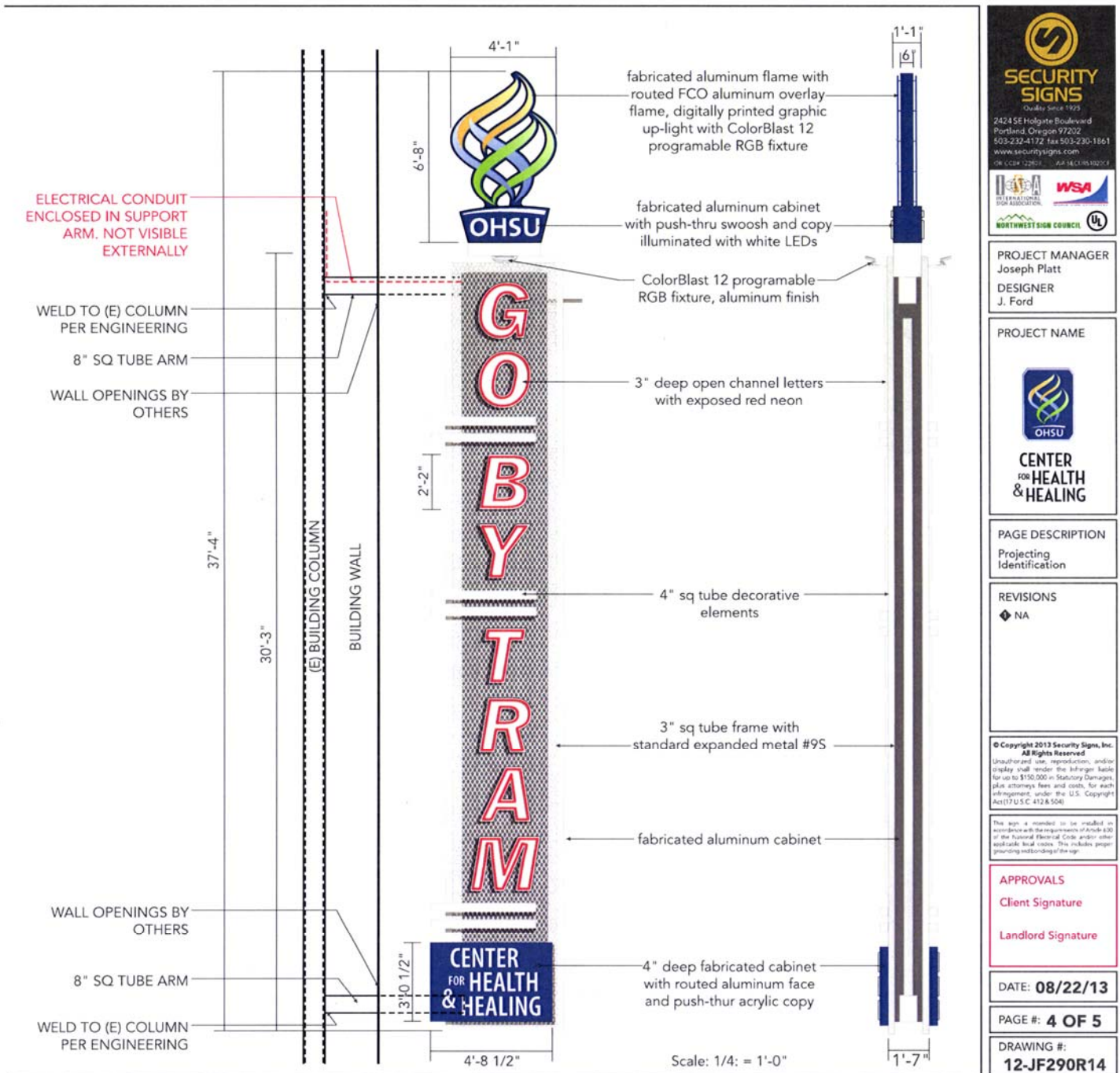
SIGN TO BE INSTALLED
AT (B) BUILDING COLUMN


(SW Moody Ave)

3303 SW Bond Avenue Sheet size has been altered.

(A) = proposed new sign

Proposed New Sign - Elevation and Section Drawings






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www.securitysigns.com

INTERNATIONAL SIGN ASSOCIATION (ISA) MEMBER
WESTERN SIGN COUNCIL (WSC) MEMBER

PROJECT MANAGER
Joseph Platt
DESIGNER
J. Ford

PROJECT NAME

CENTER FOR HEALTH & HEALING

PAGE DESCRIPTION
Projecting Identification

REVISIONS
NA

© Copyright 2013 Security Signs, Inc. All Rights Reserved.
Unauthorized use, reproduction, and/or display shall render the infringer liable for up to \$150,000 in Statutory Damages, plus attorneys fees and costs, for each infringement, under the U.S. Copyright Act (17 U.S.C. 412 & 504).

This sign is intended to be installed in accordance with the requirements of Article 406 of the National Electrical Code and/or other applicable local codes. This includes proper grounding and bonding of the sign.

APPROVALS
Client Signature _____
Landlord Signature _____

DATE: 08/22/13
PAGE #: 4 OF 5
DRAWING #: 12-JF290R14

3303 Sw Bond Avenue

Case number: LU 13-157554 DZM

Approved

City of Portland - Bureau of Development Services

Planner Kate Date October 18, 2013

* This approval applies only to the reviews requested and is subject to all conditions of approval. Additional zoning requirements may apply.

Exhibit C-5