



STAFF REPORT AND RECOMMENDATION TO THE DESIGN COMMISSION – APPROVAL

CASE FILE: LU 16-273094 DZM AD
 PC # 16-128329
 The Press Blocks

REVIEW BY: Design Commission

WHEN: March 23, 2017, 1:30 pm

WHERE: 1900 SW Fourth Ave., Room 2500A
 Portland, OR 97201

It is important to submit all evidence to the Design Commission. City Council will not accept additional evidence if there is an appeal of this proposal.

Bureau of Development Services Staff: Jeffrey Mitchem 503-823-7011 / Jeffrey.Mitchem@portlandoregon.gov

GENERAL INFORMATION

Applicant: Krista Bailey | Urban Renaissance Group
 720 SW Washington St, #630
 Portland, OR 97205
 Oregonian Publishing Co
 1621 SW Taylor St
 Portland, OR 97201

Representative: Michelle Schulz | GBD Architects, Inc
 1120 NW Couch, Suite 300
 Portland, OR 97209

Site Address: 817 SW 17TH AVE

Legal Description: BLOCK 321 LOT 1-8 LAND & IMPS SEE R646215 (R667733891) FOR MACH & EQUIP, PORTLAND; BLOCK 328 LOT 1&2&7&8 LAND & IMPS SEE R646216 (R667734621) FOR MACH & EQUIP, PORTLAND; BLOCK 321 LOT 1-8 MACH & EQUIP SEE R246773 (R667733890) FOR LAND & IMPS, PORTLAND

Tax Account No.: R667733890, R667734620, R667733891

State ID No.: 1N1E33DC 03600, 1N1E33DC 03000, 1N1E33DC 03600A1

Quarter Section: 3028

Neighborhood: Goose Hollow, contact Jerry Powell at 503-222-7173
planning@goosehollow.org.

Business District: Stadium Business District, contact Tina Wyszynski at Tina.wyszynski@gmail.com
 Goose Hollow Business Association, contact Angela Crawford 503-223-6376.

District Coalition: Neighbors West/Northwest, contact Mark Sieber at 503-823-4212.

Plan District: Central City - Goose Hollow
Zoning: CXd, Central Employment with a Design Overlay
Case Type: DZM AD, Design Review with Modifications and an Adjustment Type III, with a public hearing before the Design Commission.
Procedure: The decision of the Design Commission can be appealed to City Council.

Proposal:

The proposal is for a 3-building development on two parcels under common ownership on 1.5 blocks in the Central City – Goose Hollow Plan District identified as follows (quantities are approximate):

- **Full Block** (site area: 40,000 sf) – 1621 SW Taylor St, bound by SW Taylor St (south), SW Yamhill St (north), SW 16th Ave (east) and SW 17th Ave (west). Two buildings: **Residential Tower Building** and **Plaza Building**.
- **Half Block** (site area: 19,800 SF) – 817 SW 17th Ave, bound by SW Yamhill St (north), SW 17th Ave (east) and SW 18th Ave (west). One building: **Office Building**.

Full Block. The **Residential Tower Building** (344,351 SF, apartments over commercial/amenity space, 250’ in height) on SW 16th Avenue and the **Plaza Building** (41,500 SF, office over retail, 55’ in height) along SW 17th Avenue are located over a below-grade, shared parking garage, with a new mid-block pedestrian connector above the garage between the buildings. The garage below the full block site will accommodate approximately ±469 cars, and will be accessed from SW 16th Avenue. A bicycle mezzanine is included at the top level of the below grade garage and includes ±582 bike parking spaces and other bike related services, which are directly linked to a bike club amenity space at ground level, activating the street.

- **Design Concept.** Conceptually, the building designs are inspired by the site’s newspaper publishing history and form a contrasting, yet subtly unified relationship with one another and immediate context. A ‘press and paper’ metaphor is the primary basis for the expression of the overall project. The materiality of the two buildings recalls the materials used throughout Goose Hollow—brick, stucco/cementitious, and steel elements. The ground level of each building consists of retail, work/live, lobby entrances, and active uses to form a vibrant, active culture along the adjacent streets and open spaces internal to the block.
 - **Residential Tower Building.** As a metaphor for a published newspaper, this slender, vertical residential building features façade patterns abstracting the printed faces (west and east elevations) and edges (north and south elevations) of newsprint. This building will be a concrete structure primarily clad in window wall and a combination of textured and smooth cementitious panels. As a contrast to the brick used on the Plaza Building, the Residential Building recalls the stucco and cementitious buildings in the Goose Hollow neighborhood. The building will include approximately 2,000 SF of retail on the south end of the ground level and 337 residential units of varying sizes, plus three (3) additional work/live units reside along the Mid-block Connector. The residential use is intended for market-rate apartment housing and includes shared amenity spaces at the lower levels, as well as a rooftop view-level with amenity spaces, and rooftop deck with small trees, social spaces and gardens.
 - **Plaza Building.** As a metaphor for a mechanical printing press, and in response to the brick buildings of Goose Hollow, this mixed-use building is envisioned as a modern interpretation of an industrial edifice. The building

will be a steel and wood-framed structure clad in brick with large ground-level storefronts and punched windows on the upper office floors. Approximately 9,000 net square feet of retail space will occur on sidewalk levels. Retail and office areas will be designed to a core and shell level of completion. Three (3) two-story work/live units form the eastern edge of this building, to provide additional form and scale diversity, and to activate the mid-block connection; they contain separate living spaces above shell space below.

- **Mid-block Connector.** A mid-block pedestrian connector allows accessible passage in the north-south direction and features program complexity in order to unify the two buildings – retail spill-out spaces at retail and the ground-levels of the work/live spaces, raised planters (treating stormwater from the Residential Tower Building), pedestrian paving areas, and a diverse planting palette.
- **Public Plaza (The Hollow).** The south end of the Plaza Building pulls back from SW Taylor Street by approximately 48' to form a southwest oriented plaza (approximately 5,500 SF) that is well activated through site programming and adjacent active use. Retail / restaurant uses will be allowed to spill-out from the Plaza Building at grade and on a mezzanine-level balcony, as well as from the retail space at the south end of Residential Tower Building on the east. Included in the plaza are a water feature recalling the legacy of Tanner Creek to provide the sound and activity, a grove of trees in a moiré pattern (within storm planters and in tree grates to maximize flexibility), short term bike parking, and built-in and flexible seating.

Half Block – Office Building. The half block development consists of a single 8-story mixed-use office building of approximately 152,349 SF and 136'-6" in height (including the 13'-9" additional height for the rooftop amenity and mechanical screen enclosure.) The ground floor will consist of a combination of retail/commercial lease space (8,870 square feet) and general access/support services for the office building, including a bike storage and locker room for about 124 bicycles. The below grade structure will include one floor of parking with approximately 40 vehicular parking stalls, with a smaller second level below grade which will provide access to the tunnel that runs eastward to the Full Block below-grade parking. The upper office will include seven floors of speculative office that will allow for future single occupant tenants, or multi-tenant configurations. The rooftop will be utilized as an office amenity floor with a community room and outdoor roof deck, in addition to the screened in mechanical system area. The project's design goals and concept are:

- **Design Concept.** The design concept manifests as a reference to the site's historic communication related use (printing operations for the Oregonian Newspaper) as a means of connecting people and place as it relates to dissemination of information. The concept is rooted in the idea of communication – between people, place and history – as the basis of the design by integrating those three elements as a cultural nexus between the immediate Goose Hollow culture, the greater Portland area and beyond. That nexus is expressed through the building's exterior cladding (pre-cast concrete panel in two colors accented with honeycomb-back metal panel) represented as a frame pattern with large window openings, using the break of the standard frame rhythm to reveal larger windows. Similar to how a newspaper announces the content of its articles with large headlines and graphic images that connect the reader to the story, and the story connecting the reader to the place. The shifting of these larger openings aims to emulate the front page of a newspaper, while at

the same time, serving as a connection between the interior spaces to the surrounding neighborhood and city beyond.

Summary of Key Standards:

Floor Area Ratio (33.510.200). Both parcels have a base FAR of 6:1 + bonus maximum of 3:1 for a total allowable FAR of 9:1, or 538,200 SF for all three buildings. The proposed FAR for all three buildings is 538,200 SF.

Height (33.510.205). Both parcels have a Base Height of 250', with an additional 45' of General Bonus Height and 30' of Housing Bonus Height, for a total allowed height of 325'. All three buildings are proposed within the allocated base zoning height requirements – Building 1 at 250'; Building 2 at 55'; Building 3 at 120'.

Parking and Loading (33.510.265.E.2). There is no minimum ratio for residential parking within the Goose Hollow Subdistrict. Approximately 500 parking spaces will be provided below grade utilizing the existing 1621 SW Taylor building basement to provide a portion of the underground parking. An existing tunnel under SW 17th Ave connects the below grade levels of both parcels which will be maintained as a pedestrian connection between the two blocks. Loading spaces required for full block – 2 Standard A spaces, proposed – 2 Standard A spaces.

Bicycle Parking (33.266.220). Long-term bicycle spaces required – 531 spaces, proposed – 701 spaces. Short-term bicycle spaces required – 26 spaces, proposed – 34 spaces.

Central City Parking Review (33.510.265.F.6). A concurrent Central City Parking Review (CCPR) is required to allow motor vehicle access within 75' of a Light Rail Alignment. At the time of the publication of this Staff Report, the scope of work had been approved by PBOT and the analysis was underway. In the event that CCPR recommendations impact the design of this subject proposal, the following Conditions of Approval are required:

- *The Central City Parking Review shall be approved prior to the issuance of any building permits.*
- *Should the Central City Parking Review require changes to the design of the proposal, a follow-up Type II LUR shall be required.*

Modifications. Three (3) Modifications to Portland Zoning Code standards (Title 33) are requested:

1. **Ground Floor Windows in the CX Zone (33.130.230).** Required – 50% of ground floor wall length must contain windows with views into working areas. Proposed – the east elevation of the half block building proposes windows on approximately 43'-9" (approximately 44%) of the ground floor length.
2. **Ground Floor Active Use (33.510.225).** Required – 50% of the ground floor wall length must contain active uses. Proposed – the east elevation of the half block building proposes active use for 39'-6" (approximately 40%) of the ground floor wall.
3. **Bike Parking Stall Width (33.266.220.C3).** Required – a space 24" x 6' must be provided for each required bicycle parking space. Proposed – 18" spacing for hung racks with a 6" vertical stagger.

Adjustments. One (1) Adjustment to Portland Zoning Code standards (Title 33) is requested:

1. **Quantity of Loading Spaces (33.266.310.C.).** Two on-site loading spaces meeting Standard A are required for the Office Building on the half block site. The project proposes one Standard A loading space abutting SW 17th Ave. *Loading/Queuing Analysis under review by PBOT.*

Design Exception. One (1) Design Exception (DE) is requested:

- Garage Door Setback – 20' required, 4'-6' proposed. *DE under review by PBOT.*

Central City Parking Review (CCPR). A concurrent CCPR is underway for the proposed location of loading access within 75' of a light rail alignment pursuant to Portland Zoning Code Chapter 33.510.265.F.6. The CCPR consists of the following scope of work

- Turning movements associated with truck ingress/egress and the potential effect a circulating truck may have on pedestrians, cyclists and the light rail operations.
- Assessment of adequacy of services as well as other general CCPR criteria including intersection capacity at all abutting intersections.

In order to ensure compatible review, the following Conditions of Approval to this subject Land Use Review will be necessary

- *The CCPR shall be approved prior to the issuance of any building permit.*
- *If complying with CCPR requirements necessitates changes to the design that do not conform substantially with this subject Land Use Review, a follow up Land Use Review will be required.*

Design Review is necessary because the proposal is for new construction within a Design Overlay Zone.

Approval Criteria:

In order to be approved, this proposal must comply with the approval criteria of Title 33, Portland Zoning Code. The applicable approval criteria are:

- Central City Fundamental Design Guidelines
- 33.825.040 Modification Review
- Goose Hollow Design Guidelines
- 33.825 Design Review
- 33.805 Adjustments

ANALYSIS

Site and Vicinity: The site currently supports two Oregonian Publishing buildings to be demolished. Neighboring development includes a variety of buildings ranging in age – eastward on SW 15th Avenue the 1924 3-story Lownsdale Apartments, the recently constructed North Hollow Apartments (5 over 1 market rate apartments featuring ground level residential and retail at the corner of SW 15th Ave and Taylor St) and the recently approved contemporarily expressed 11-story 1500 SW Taylor Apartment Building and the 7-story 1440 SW Taylor St Apartments. Significant community landmarks nearby include Lincoln High School one block south, Portland Timbers home pitch (Providence Park) located across SW 18th Ave to the west and the sunken I-405 freeway two blocks east. SW 16th Ave, SW 17th Ave, SW 18th Ave are Local Service Bikeways. SW Taylor is a City Bikeway and Traffic Access Street. SW Yamhill is a City Walkway and Regional Transitway and Major Transit Priority Street.

The policy context framing the vision for the future of the Goose Hollow Neighborhood is set forth in the Goose Hollow Station Community Plan (circ. 1995). This plan was developed in accordance with the findings and conclusions of previous inter-agency (Metro, Tri-met, City of Portland) planning efforts dating to the 1980s including, but not limited to Westside Light Rail Station Community Planning, the Central City Plan, Central City Transportation Management Plan, Transit Station Area Planning Program, Livable City Project and the Tanner Creek Basin Project. These planning efforts were predicated on maximizing community development, increasing transportation mode choice and transit ridership, reducing auto use in the Central City, enhancing character, and encouraging housing and commercial development compatible with the growing community. Key visioning attributes for the Civic Station area (within which the subject site lies) include up-zoning the core of the station area (including the subject site) to the most urban and intense in Goose Hollow and the Central City (CX), creating

the largest maximum heights and Floor Area Ratios in the Goose Hollow Neighborhood, and establishing required residential development areas.

The physical context of this portion of the Goose Hollow neighborhood features a dynamic mix of uses, public activity, transportation options and architectural styles. Built form ranges in style from pre-war masonry apartments and concrete commercial low-rise to contemporary glass window wall high-rise development. Recently approved vicinity projects by the Portland Design Commission feature program and design character similar to that proposed by the subject project – mixed-use, market-rate apartments, contemporary expressions with predominant glazing and traditional materials palette (masonry, cementitious, etc.), and active ground levels.

The proposed project responds to this context with pervasive active ground-level program, shared below-grade parking, eroded building frontages, mid-block accessible connection between SW Yamhill St and SW Taylor St, an actively programmed public plaza, and diverse, yet unified architectural design that interprets historic context in contemporary expression and enhances architectural character of the broader neighborhood.

Zoning: The Central Commercial (CX) zone is intended to provide for commercial development within Portland's most urban and intense areas. A broad range of uses is allowed to reflect Portland's role as a commercial, cultural and governmental center. Development is intended to be very intense with high building coverage, large buildings, and buildings placed close together. Development is intended to be pedestrian-oriented with a strong emphasis on a safe and attractive streetscape.

The Design Overlay Zone [d] promotes the conservation, enhancement, and continued vitality of areas of the City with special scenic, architectural, or cultural value. This is achieved through the creation of design districts and applying the Design Overlay Zone as part of community planning projects, development of design guidelines for each district, and by requiring design review. In addition, design review ensures that certain types of infill development will be compatible with the neighborhood and enhance the area.

Land Use History: City records indicate there are no prior land use reviews for this site.

Agency Review: A *Notice of a Public Hearing on a Proposal in Your Neighborhood* was mailed on February 10, 2017. The following Bureaus have responded with no issue or concerns:

- Site Development (Exhibit E.1)
- Fire Bureau (Exhibit E.2)
- Bureau of Environmental Services (Exhibit E.3)
- Life Safety (Exhibit E.4)
- Water Bureau (Exhibit E.5)
- Portland Bureau of Transportation (Exhibit E.6)

Neighborhood Review: A *Notice of a Public Hearing on a Proposal in Your Neighborhood* was mailed on February 10, 2017. A total of one written response has been received from either the Neighborhood Association or notified property owners in response to the proposal.

- Tracy Prince, email dated December 14, 2017 (Exhibit F.1). Comments in opposition to the project citing ground floor windows and ground floor active use Modifications.

Staff Response: Please refer to Section (2) of this Staff Report describing how the project better meets approval criteria as required by Portland Zoning Code Chapter 33.825.040 in order to merit this Modification.

- Michael Mehaffy, President, Goose Hollow Foothills League (Exhibit F.2). Comments in opposition to the project citing the following:

- Design Goal 4 states, “Preserve and enhance the community’s history and architectural character.” However, a building that is significantly taller than surrounding buildings, and architecturally insensitive to its context, is likely to detract from, not enhance, existing architectural character. This is the case with the proposed scheme.

Staff Response: Design Goal 4 explicitly states that preserving and enhancing the community’s history and architectural character can be achieved through integrating the history of the Community, symbolically, with its natural and formal (man-made) features. No explicit or implied reference is made to building height as an element of community character to preserve. Design Goal 4 is not an approval criterion, but rather one of four guiding goals for the approval criteria (Goose Hollow District Design Guidelines).

Please refer to Section I Design Guidelines below for detailed findings related to the applicable approval criteria.

- A5-1 states, “Strengthen the identity of the Civic Stadium station area,” and “d. using architectural vocabulary and materials that maintain continuity with existing developments...” However, the architectural vocabulary in the proposal in terms of massing, does not maintain continuity with existing developments. Indeed it is abruptly discontinuous.

Staff Response: This guideline’s background statement reads “The scale and character of new mixed-use and residential development should respect the mid- to high-rise scale of the existing buildings within the station area.” As proposed, the project’s three buildings of varying height (low-55’, mid-136’ and high-250’), simple proportions, contemporary expression and locally germane materials respects the mid- to high-rise scale of existing buildings while adding to the adding to the character of the station area.

The Residential Tower responds to this guideline in height, form, architectural character and active ground level which, on balance, will strengthen the identity of the existing Civic Station Area neighborhood focal point – both as visual marker and public gathering place – as called for in the guideline.

Please refer to Section I Design Review below for detailed findings related to this guideline.

- B1 states, “provide human scale to buildings along walkways.” However, the portion of the building that is at approx. 24 stories appears to loom over NW 16th Avenue for a full block, and over Yamhill and Taylor for approx. 1/2 block, with no apparent step-back or other effort to provide human scale to the building along the walkway, in terms of massing. It is therefore disruptive to human scale along the walkways.

Staff Response: Guideline B1 regulates the building frontage zone to reinforce and enhance the pedestrian system through design elements at the sidewalk-level of buildings. The proposed residential tower features frontage zone design articulation that reinforces human scale through the following measures

- *Strong horizontal datum at approximately 20’ above sidewalk grade,*
- *Recessed (approximately 4’) retail bays framed by pilasters,*
- *Continuous canopies (8’ deep) at approximately 12’ above sidewalk grade,*
- *Residential unit balconies at approximately 32’ above sidewalk grade.*

- 2-level work/live units (with roll-up doors) oriented to the plaza,
- Large expanses of clear storefront glazing with visual and physical connections into active floor area;
- Well-detailed storefront and textured wall surfaces with articulated steel detailing at the windows, and steel and stone elements at the column bases.

Please refer to Section I Design Review below for detailed findings related to this guideline.

- o B6 states, “consider sunlight, shadow, glare reflection, wind, and rain.” The proposed building masses will cast unnecessarily large mid-day and afternoon shadows on the blocks to the east and north. They may also create significant wind patterns on NW 16th.

Staff Response: Guideline B6 explicitly states “Develop integrated weather protection systems at the sidewalk-level of buildings to mitigate the effects of rain, wind, glare, shadow, reflection, and sunlight on the pedestrian environment.” The operative regulatory intent of this guideline regards the size and placement of specific building elements at the sidewalk-level that enhance the pedestrian environment (with the exception of sun shading at upper levels). The guideline is not intended to regulate shadow or wind impacts resulting from building massing.

Please refer to Section I Design Review below for detailed findings related to this guideline.

- o C4 states, “Complement the context of existing buildings.” The context here includes the scale and massing of existing buildings, which are a distinctive characteristic of the Goose Hollow neighborhood. This proposal introduces a starkly discordant height and form, more characteristic of the suburban “Le Corbusier” style of slab building, and far from a graceful, context-sensitive urban form.

Staff Response: Guideline C4 requires that new buildings embody “design characteristics reflected in an area’s design vocabulary including building proportion, scale, rhythm, and construction materials, as well as smaller-scale elements such as window and/or door styles, color and roof shape.” The Goose Hollow District Design Guidelines are clear in describing an Urban Design Vision for the future of this part of the neighborhood (Civic Stadium Station) as a main neighborhood focal point characterized by dense mid- and high-rise residential developments that complement context.

The existing and emerging context of this station area is eclectic, ranging from traditional masonry mid-rise residential structures to contemporary infill high-rise development. This diversity is referenced by the proposal through variety in building proportion, scale and materials. The Plaza and Office Buildings relate in height and materiality to the nearby historic masonry apartment buildings in the neighborhood (the Multnomah Athletic Club and some of the cementitious mid-century high rises further west.) The Residential Tower references an evolving trend toward contemporary high-rise infill (north and east of the site) through height, program, and materials, to contribute to the diversity of Goose Hollow as called for in the guideline. All buildings share several unifying features referencing context – ground level active use, transparent storefronts, ample pedestrian amenities and well-detailed cementitious cladding material.

Regarding the Le Corbusier invocation, three significant departures from the principles of Corbusian brutalism render the proposed design compatible with approval criteria – (1) a well-engaged ground plan, (2) a humane ground level program with human-scale design detail, and (3) well-glazed window wall composed of vertically expressed façade elements.

Please refer to Section I Design Review below for detailed findings related to this guideline.

Procedural History. The subject proposal was heard before two Design Advice Requests (August 25, 2016 and October 20, 2016.) As mentioned above, the proposal requires a concurrent Central City Parking Review because loading access to the half-block building from SW 17th Ave is less than 75' from the SW Yamhill Ave ROW (light rail alignment.) Following, is a summary of that procedural history:

DAR #1 – August 25, 2016 (Commissioners present: Livingston, Wark, Savinar, Molinar, Vallaster)

Executive Summary:

The majority of the Commissioners present were generally comfortable with the proposed height, massing and tower orientation. However, two of the five Commissioners present had reservations regarding the tower height and tower orientation and additional information would be critical to further evaluate these aspects of the proposal.

Specific Discussion:

1. **Building height & massing**

- a. Full Block 250' residential tower at full block + approx. 60' low rise office building
- b. Half Block approx. 120' office tower
- c. Mechanical penthouse setback

2. **Tower Orientation**

- a. Analysis of North-South vs. East-West
- b. Conclusion of North-South direction
 - The majority of Commissioners present were comfortable with the building heights and massing as proposed.
 - The same Commissioners supported the north-south tower orientation as the shadow studies would have less impact on the neighborhood and the Yamhill-Taylor pedestrian connection would be very successful.
 - Two Commissioners however expressed concern and are less convinced that the heights and massing are approvable, particularly of the tower building, and additional information is necessary.
 - Commissioners expressed concern that the east-west orientation of the tower could be a stronger concept in that it connects important urban conditions [such as east-west oriented SW Taylor Street Light-Rail Transit Street, downtown views from the west, and overall massing orientation east towards the river] from the upper Goose Hollow and Kings Hill neighborhoods to the west, to the downtown core and river to the east. Additional information such as longer west-east sections across these neighborhoods may help allow for more conclusive advice.
 - Additional information that shows the full-build out of the district, with FAR, heights and bonus heights considered, would also be helpful [this was requested of City Staff and will be provided at a future date].
 - Consideration of additional residential floor area within the sidecar building [increasing its height] could be a solution to help reduce the height of the residential tower.
 - The first 40' of the 250' tower's architecture must be highly developed to reflect the neighborhood's architectural scale.
 - Some Commissioners expressed concern of the scale of a 250' tower adjacent to the 30' wide open space.

- The mechanical penthouse of the tower will need to be well integrated with the massing.
 - Regarding the half block, some Commissioners expressed support for a bigger move at the prominent NW corner. This entire corner, not just the ground plane or upper story, has a high degree of exposure, presence and visibility from Providence Park and MAX that demands a larger response.
3. **Site Design Concepts – Traffic & Loading**
- a. SW 17th & SW Yamhill intersection redevelopment
 - b. Active uses [Bike Club and Parking entry & Loading locations]
 - c. Pedestrian through block connector
 - d. Open space
- There are a number of extremely strong moves these buildings are doing at the streetscape in terms of activating the street: increasing sidewalk widths, adding active spaces, fixing the Tri-Met stop, and adding more landscaping.
 - Commissioners agreed the work-live proposal was excellent.
 - Commissioners expressed concern with uses proposed from the corner of SW 16th and SW Taylor Street, a leasing office, and the adjacent spaces along SW 16th Avenue as not being active enough for this critical frontage.
 - Commissioners supported the plaza and pedestrian connectors as a very strong concept.
4. **Materials and exterior character**
- a. Elevation studies
 - b. Materials concepts and quality
- Materials will be critical, as will the development of detailing and richness to the final design's architecture in order to respond to the context of the Goose Hollow neighborhood. This is particularly true for the proposed 250' tower.
 - The architecture of the buildings will need to pick up on the richness of the materials in the neighborhood. Oozing with character would be a good bar for this new construction.
 - Continue the strong moves of cut-outs and architectural richness of the proposed buildings.

DAR #2 – October 20, 2016 (Commissioners present: Livingston, Wark, Clark, Molinar, Vallaster)

Executive Summary:

Design Commissioners present were unanimous in their support of the full-block proposal regarding the height, massing and north-south tower orientation. The resulting open space created, particularly the ground level activation and work-live programming, was also supported unanimously.

Specific Discussion:

1. **Building height & massing**

- a) Full Block 250' residential tower at full block
- Commissioners commented that the 250' height limit is allowed, and this is a good location for high-density construction.
 - Another Commissioner commented that the Applicant's building massing diagram makes a lot of sense, particularly the visioning of the potential future build out of this neighborhood.

2. **Tower Orientation**

- a. Analysis of North-South vs. East-West
 - b. Conclusion of North-South direction
- The Commission fully supported a north-south tower orientation.
 - Commissioners stated that the nature of building construction will produce various massing options and orientations, and will ultimately weave together as this Central City sub District grows towards its potential.

- One Commissioner responded that an east-west orientation would hurt the open space potential of this full block.
- 3. **Open space**
 - Commissioners commented that the mid-block connector is great, and the work-live is fantastic.
 - As for work-live, Design Commission is excited to see the program develop a successful ground floor design that can raise the bar in Portland.
 - Commission encourages the full-block design team to emphasize the difference in texture of these two buildings.

ZONING CODE APPROVAL CRITERIA

I. DESIGN REVIEW (33.825)

Section 33.825.010 Purpose of Design Review

Design review ensures that development conserves and enhances the recognized special design values of a site or area. Design review is used to ensure the conservation, enhancement, and continued vitality of the identified scenic, architectural, and cultural values of each design district or area. Design review ensures that certain types of infill development will be compatible with the neighborhood and enhance the area. Design review is also used in certain cases to review public and private projects to ensure that they are of a high design quality.

Section 33.825.055 Design Review Approval Criteria

A design review application will be approved if the review body finds the applicant to have shown that the proposal complies with the design district guidelines.

Findings: The site is designated with design overlay zoning (d), therefore the proposal requires Design Review approval. Because of the site's location, the applicable design guidelines are the Central City Fundamental and Goose Hollow Special Design Guidelines.

Goose Hollow District Design Guidelines and Central City Fundamental Design Guidelines

The Goose Hollow District is envisioned to be a predominantly urban residential, transit-oriented community located on the western edge of the Central City between Washington Park and Downtown Portland. When riding light rail through the West Hills tunnel to the Central City, it is the first neighborhood experienced before entering downtown Portland. The Urban Design Vision celebrates the sense of arrival from the west at Jefferson Street Station and Collins Circle, and from the north at the Civic Stadium Station and Fire Fighter's Park. This is done by integrating the history of the community with its special natural and formal (man-made) characteristics.

The Central City Fundamental Design Guidelines focus on four general categories. **(A) Portland Personality**, addresses design issues and elements that reinforce and enhance Portland's character. **(B) Pedestrian Emphasis**, addresses design issues and elements that contribute to a successful pedestrian environment. **(C) Project Design**, addresses specific building characteristics and their relationships to the public environment. **(D) Special Areas**, provides design guidelines for the four special areas of the Central City.

Goose Hollow District Design Goals

The Goose Hollow District Design Goals are specific to the Goose Hollow District. These urban design goals and objectives are to:

- Enhance mixed-use, transit-oriented development around the light rail stations to make it a pedestrian-friendly station community.
- Provide open spaces to accommodate active public life.
- Strengthen connections to adjacent neighborhoods through light rail, bike and pedestrian access and assure a safe and pleasant bike/pedestrian environment.
- Preserve and enhance the community's history and architectural character.

Central City Plan Design Goals

This set of goals are those developed to guide development throughout the Central City. They apply within all of the Central City policy areas. The nine goals for design review within the Central City are as follows:

1. Encourage urban design excellence in the Central City;
2. Integrate urban design and preservation of our heritage into the development process;
3. Enhance the character of the Central City's districts;
4. Promote the development of diversity and areas of special character within the Central City;
5. Establish an urban design relationship between the Central City's districts and the Central City as a whole;
6. Provide for a pleasant, rich and diverse pedestrian experience for pedestrians;
7. Provide for the humanization of the Central City through promotion of the arts;
8. Assist in creating a 24-hour Central City which is safe, humane and prosperous;
9. Ensure that new development is at a human scale and that it relates to the scale and desired character of its setting and the Central City as a whole.

Staff has considered all guidelines and has addressed only those guidelines considered applicable to this project.

A1. Integrate the River. Orient architectural and landscape elements including, but not limited to lobbies, entries, balconies, terraces, and outdoor areas to the Willamette River and Greenway. Develop access ways for pedestrians that provide connections to the Willamette River and Greenway.

A2. Emphasize Portland Themes. When provided, integrate Portland-related themes with the development's overall design concept.

A2-1. Recognize the Historic Tanner Creek Theme. Recognize the course of the historic Tanner Creek and emphasize the District's connection with the Creek on site developments of 20,000 square feet or more, including and immediately adjacent to the historic course of the Creek. This guideline may be accomplished by any or all of the following:

- a. Exposing the Creek using water features and fountains; or
- b. Incorporating interpretive trails, artwork, murals or sculptures that describe and symbolize the relation between the district and the history of Tanner Creek.

A5-5. Incorporate Water Features. Incorporate water features or water design themes that enhance the quality, character, and image of the Goose Hollow District.

Findings for A1, A2, A2-1, and A5-5: Integrate the River. The site is located generally equidistant to the Willamette River in both northeast and southeast directions. On the Full Block site, river views will be possible from the east and north residential units as well as the roof terrace of the Residential Tower Building. On the Half Block site, external westward vantage points are provided at level 7 and the roof deck. In addition, improved streetscapes will create a pleasant pedestrian experience along streets connecting east to downtown and the river.

Emphasize Portland Themes. Portland themes will primarily be integrated through the fundamental concept for the project – “Press + Paper”, relating to site’s former use as the printing operation of the Oregonian newspaper – as follows:

- The Residential Tower Building conveys the complexity of newspaper pages – the north and south elevations mimicking the lightness and verticality of pages, while the east and west elevations convey the proportions of the print page with type-face and inset pictures.
- The Plaza Building embodies the printing press with expressed structure and raw materials (brick and steel).
- The Office Building pays homage The Oregonian’s printing press and paper storage building both in concept and materiality – conceptually, the building serves as a means of communication and connection with the neighborhood (much like a newspaper) and in materiality, by using the same material the Oregonian’s Printing press structures used – precast concrete, a material widely used in the Goose Hollow neighborhood. The building also adds the warmth of wood in a few featured locations, including along the sidewalk – wood being such a prevalent material in the City, with strong historical roots.

Recognize Tanner Creek. While Tanner Creek now runs in a channel deep below SW 16th avenue, the historic course was actually south to north a few blocks to the southwest. The proposed site design references Tanner Creek with a series of waterways connecting the Residential Tower Building’s roof runoff across the Mid-block Connector in trench drains into stormwater planters along the Plaza Building. A water feature is also included in the Plaza. The design of the Office Building also incorporates warm wood elements at the pedestrian level as an ode to the history of Tanner Creek with the dense growth of Hemlock trees along its bank.

Therefore, these guidelines are met.

A3. Respect the Portland Block Structures. Maintain and extend the traditional 200-foot block pattern to preserve the Central City’s ratio of open space to built space. Where superblocks exist, locate public and/or private rights-of-way in a manner that reflects the 200-foot block pattern, and include landscaping and seating to enhance the pedestrian environment.

A4. Use Unifying Elements. Integrate unifying elements and/or develop new features that help unify and connect individual buildings and different areas.

C4. Complement the Context of Existing Buildings. Complement the context of existing buildings by using and adding to the local design vocabulary.

Findings for A3, A4, and C4: *Respect Block Structure.* Fully resolved as a unified whole, both blocks of the project will be built to reinforce the existing block structure with street level frontages of a mix and variety of active uses. The Full Block site will feature a dynamic mix of public and private space programming that will diversify street life and create a significant focal point in the neighborhood – active ground level uses, an eroded sunny southwest corner, accessible pass-through connectivity, and passive and active open space elements. The Half Block site is built to the property lines with the exception of the northwest corner where the ground-level building frontage is recessed in response to the ROW pinch point resulting from the MAX turn and to allow retail spill-out at this important gateway corner.

Unifying Elements. The project will create a unified streetscape with established street elements such as historic light standards, new street trees, as well as consistent sidewalk joint patterns, site furnishings and short term bike parking. The project's overall material palette continues that found in Goose Hollow, and each building is unified with its immediate context. Additionally, the site design will help to clearly articulate the TriMet station zone emphasizing the area's rich transit access. A water feature in the Plaza recalls Tanner Creek, and an artistic crosswalk riffs on the rich vibe of the Goose Hollow neighborhood. Embellished modern steel elements at the base of both buildings will complement the character of the development as a whole.

Compliment Context. The Goose Hollow District Design Guidelines are clear in describing an Urban Design Vision for the future of this part of the neighborhood (Civic Stadium Station) as a main neighborhood focal point characterized by dense mid- and high-rise residential developments that complement context. The existing and emerging context of this station area is eclectic ranging from traditional masonry mid-rise residential structures to contemporary infill high-rise development.

This contextual diversity is referenced by the proposal through variety in massing, scale, ground-level program and materials. The Plaza and Office Buildings relate in height and materiality to the nearby historic masonry buildings in the neighborhood including nearby apartment buildings, the Multnomah Athletic Club and some of the cementitious mid-century high rises further west. The Residential Tower reflects an evolving trend toward contemporary high-rise infill through height, program, and materials, to contribute to the diversity of Goose Hollow and serve as a neighborhood focal point – both as visual marker and public gathering place. All buildings share several unifying architectural details referencing context – ground level pedestrian amenities such as canopies, material detail and transparent storefronts; similar rhythm of storefront bays lining the sidewalks and the Mid-block Connector (unifying the full length of the connector through active use and pervasive inside-outside connections.)

Context is also rich with pedestrian amenities and urban design elements supportive of a high-density, mixed-use, transit-supportive neighborhood. The project's public space and streetscape design dedicate significant amounts of ground plane to common use. The Mid-Block Connector, plaza, recessed storefronts, eroded corners and scoring patterns will unify the project with the neighborhood's pervasive public amenities. Special attention has been paid to the corner at SW18th and Yamhill, where the MAX currently turns and creates a precarious and tight condition for the pedestrian. The proposed design pulls the building's corner façade in at the street level to relieve the pressure created by the MAX while providing the pedestrian additional safety and protection. This recess unifies public and private realm with transitional activities such as outdoor seating and gathering while maintaining functional pass-through space.

Therefore, these guidelines are met.

A5. Enhance, Embellish, and Identify Areas. Enhance an area by reflecting the local character within the right-of-way. Embellish an area by integrating elements in new development that build on the area's character. Identify an area's special features or qualities by integrating them into new development.

A5-1. Strengthen the Identity of the Civic Stadium Station Area. This guideline may be accomplished by any or all of the following:

- a. Integrating the Civic Stadium and Multnomah Athletic Club into the station area. This can be done by making their ground level street walls more pedestrian-friendly.
- b. Incorporating the history of the Stadium and Tanner Creek in the form of art work, murals, and other design features;
- c. Strengthening the neighborhood focal point located within the station area. Street trees, awnings, lighting and other amenities can emphasize pedestrian connections to the focal point (the Park, Plaza/Station). Buildings can orient their openings and entries towards the focal point to create a sense of enclosure and enhance the sense of entry into the District; or
- d. Using architectural vocabulary and materials that maintain continuity with the existing developments and add to the character of the station area.

A5-6. Incorporate Works of Art. Incorporate works of art or other special design features that increase the public enjoyment of the District.

Findings for A5, A5-1 and A5-6. The project's massing variety, scaling shifting, pervasive ground-level active uses, eroded corners, focal points and pedestrian amenities enhance local character, and strengthen station area identity and artistic expression in the following ways:

- The proposed height variety (low-55', mid-156' and high-250'), simple massing and diversity of materials respects the mid to high rise scale of existing buildings while adding to the character of the station area;
- The Residential Tower (75' lower than the maximum allowed height) references the high-rise end of the height scale maintaining continuity with existing and recently approved buildings (ranging in height from 175' to 220');
- The Residential Tower's height, form, architectural character and active ground level will strengthen the identity of the Civic Station Area by serving as a neighborhood focal point – both as visual marker and public gathering place – as called for in the guideline;
- Tanner Creek is celebrated with a series of connections between the Residential Tower Building's roof runoff across the Mid-block Connector in trench drains into stormwater planters along the Pavilion building, and a water feature is included in the Plaza;
- Public plaza at the corner of SW Taylor St and SW 17th Ave with potential for outdoor dining and gathering areas;
- The Office Building's façade erodes toward Providence Park (Civic Stadium) at the ground level and provides a visual connection between the two structures;
- The existing TriMet platform will be complemented through the use of comparable materials in the abutting Office Building;
- Pedestrian-friendly work-live units with ground-level roll-up doors oriented to the mid-block connector;
- Architectural vocabulary and materiality maintaining continuity with existing identity as follows:
 - contextual diversity is referenced through variety in massing, scale and materials,
 - the Plaza and Office Buildings relate in height and materiality to the nearby historic masonry buildings in the neighborhood,
 - the Residential Tower adds to the character of the station area with a contemporarily expressed high-rise reinforcing context through active ground-level program, publicly accessible focal points and contextually germane cementitious materials.
- Artistic elements have been incorporated into the project –
 - playful and artistic pedestrian crossing along SW Yamhill between the full and half blocks,
 - a sculptural water feature in the mid-block connector which will pay

- o homage to the history of the site and Tanner Creek,
- o Plaza Building green roof composed of growing media representing the historic course of the Willamette River,
- o a new curb extension at 17th Ave and Yamhill will feature an artful, neighborhood-themed crosswalk;
- o decorative custom metal garage/loading doors evocative of print patterns.

Therefore, these guidelines are met.

A6. Reuse/Rehabilitate/Restore Buildings. Where practical, reuse, rehabilitate, and restore buildings and/or building elements.

Findings for A6: The project is not proposing to retain the existing Oregonian Printing Plant and Paper Storage buildings. This early 1970's industrial building contains materials and elements that are impractical for reuse in the new buildings. The concrete panels that make up most of the façade are pre-stressed panels that cannot be cut or modified without severely damaging the integrity of the material. However, the development is taking advantage of existing below-grade structures and readapting them for storage and parking use.

Therefore, this guideline is met.

A7. Establish and Maintain a Sense of Urban Enclosure. Define public rights-of-way by creating and maintaining a sense of urban enclosure.

Findings for A7: To contribute to Goose Hollow's sense of urban enclosure, all structures will be built to their lot lines with minor erosions to provide a defined edge for pedestrian activities. The Plaza Building is set back from SW Taylor St to create a south-facing plaza (approximately 5,500 SF) to serve as a neighborhood gathering place; and the Office Building is eroded at the SW18th and Yamhill corner, where the façade at the ground floor is carved slightly in order to add more space to the pedestrian realm at the pinch point caused by light rail alignment. All upper-level facades meet the lot line fully on all street frontages, defining the street wall that creates the public zone.

Therefore, this guideline is met.

A8. Contribute to a Vibrant Streetscape. Integrate building setbacks with adjacent sidewalks to increase the space for potential public use. Develop visual and physical connections into buildings' active interior spaces from adjacent sidewalks. Use architectural elements such as atriums, grand entries and large ground-level windows to reveal important interior spaces and activities.

C6. Develop Transitions between Buildings and Public Spaces. Develop transitions between private development and public open space. Use site design features such as movement zones, landscape elements, gathering places, and seating opportunities to develop transition areas where private development directly abuts a dedicated public open space.

C7. Design Corners that Build Active Intersections. Use design elements including, but not limited to, varying building heights, changes in façade plane, large windows, awnings, canopies, marquees, signs and pedestrian entrances to highlight building corners. Locate flexible sidewalk-level retail opportunities at building corners. Locate stairs, elevators, and other upper floor building access points toward the middle of the block.

C9. Develop Flexible Sidewalk-Level Spaces. Develop flexible spaces at the sidewalk-level of buildings to accommodate a variety of active uses.

Findings for A8, C6, C7 and C9: Providing a diversely programmed mix of uses and pedestrian-friendly design solutions will ensure a variety of human activity throughout the day and night. As detailed below, the project employs well-resolved place-making strategies and design elements that will induce ground-level vibrancy and flexibility, and wide ranging type and duration of human activity as encouraged by these guidelines.

Residential Tower Building.

- Residential lobby/leasing, retail, commercial uses oriented to abutting sidewalks on all frontages;
- Recessed storefront bays;
- Well-glazed, corner- and plaza-oriented retail entries and canopies;
- Primary building entry oriented to corner of mid-block connector;
- Work-live units feature roll-up doors fronting the plaza;
- Built-in seating within the retail openings on SW Taylor St;
- Active “portals” – canopied residential entrance into the transparent two-story lobby at the north end of the Mid-block Connector, plaza opening at the southwest;
- Bicycle amenity space (mid-block on SW 16th Ave) with resident lounge, workshop, and access to bicycle storage in the garage level below; and,
- Retail spill-out at plaza.

Plaza Building.

- The public plaza and streets are fronted with active interior spaces and ample storefront glazing at the ground level;
- Corner- and plaza-oriented entries and canopies;
- A two-story transparent retail space with a dedicated entrance at the intersection of SW 17th Avenue and Yamhill Street;
- The sidewalk-level of the Plaza Building is designed to be flexible, as tenant spaces fronting SW 17th Avenue can be demised into smaller areas north-to-south;
- Primary building entry oriented to the plaza;
- Work-live units feature roll-up doors fronting the plaza;
- Large operable storefronts and a mezzanine-level balcony facing the pedestrian Plaza; and,
- The plaza and balcony are flexibly designed to host outdoor dining activities and to serve as a neighborhood-gathering place, as well as a transition space inviting people to use the Mid-block Connector.

Mid-block Connector.

- Canopies and layered landscape treatments providing transitions between the public walking areas and the work/live units lining the Mid-block Connector;
- The work/live units are enhanced with garage doors to provide direct connection to the midblock connection;
- Un-demised ground level spaces to maintain flexibility for maker space use, restaurant spill-out, office seating, or other activities to enhance activity; and,
- A richly layered sequence of movement zones, landscape elements, gathering places, and seating opportunities throughout the length of the space.

Office Building.

- Large transparent operable storefront allowing with visual and physical connections between interior spaces and the sidewalk;

- Active retail uses along the majority of the SW 18th Ave and SW Yamhill frontages;
- The main office entry mid-block on Yamhill, facing the MAX stop, featuring large glass canopy and glazed open lobby adds a level of entry hierarchy;
- The ground floor façade is recessed at the corner to creating additional opportunities for retail spill-out activity;
- A comparable recessed expression is repeated at the 7th floor cutout at the same corner, carrying the move vertically to highlight the corner at a larger urban scale;
- The northeast corner of the building is accentuated by multiple entry points into the retail space that face both abutting sidewalks; and,
- Concentrated service areas, loading/parking access and circulation core elements mid-block on SW 17th Ave (southeast corner).

Therefore, these guidelines are met.

B1. Reinforce and Enhance the Pedestrian System. Maintain a convenient access route for pedestrian travel where a public right-of-way exists or has existed. Develop and define the different zones of a sidewalk: building frontage zone, street furniture zone, movement zone, and the curb. Develop pedestrian access routes to supplement the public right-of-way system through superblocks or other large blocks.

B1-2. Orient Building Entries to Facilitate Transit Connections. Orient primary building entries at pedestrian circulation points which conveniently and effectively connect pedestrians with transit services.

Findings for B1 and B1-2: These guidelines seek to ensure that doors, windows, and wall and ground-plane treatments adjacent to pedestrian ways relate and contribute to pedestrian interest and enjoyment. As a whole, proposed treatments such as scale variety and building element proportions, skin materials and details, and active programming at the ground levels of all buildings provide human scale and relationships that are transit supportive by meeting pedestrian needs and interests. Key components of the design include:

Residential Tower Building.

- Human scale is reinforced through the following measures
 - strong horizontal datum at approximately 20' above sidewalk grade,
 - recessed (approximately 4') retail bays framed by pilasters,
 - continuous canopies (8' deep) at approximately 12' above sidewalk grade,
 - 2-level work/live units (with roll-up doors) oriented to courtyard.
- Large expanses of clear glazing with visual and physical connections into active floor area;
- Well-detailed canopies and textured wall surfaces along with articulated steel detailing at the windows, steel and stone elements at the rhythmic column bases; and,
- Residential unit balconies at approximately 32' above sidewalk grade.

Plaza Building.

- Tall (20'-22') ground-level storefronts and ceilings, canopies, and folding storefront walls;
- Well-detailed canopies, articulated steel detailing at the windows, and brick coursing;
- A level 2 amenity
- A scale-reducing two-story element fronting the Mid-block Connector is expressed in steel and steel panels; and,

- Amenity space and green roof on the two-story introduces pedestrian scale at approximately 20' above sidewalk grade.

Mid-block Connector.

- Detailed paving and landscape textures at varied scales;
- Layered public space zones – spill-out, clear pedestrian passage, planting, building frontage, sidewalk furnishing; and,
- Frequent entries into active frontage for the entire length of the space with over-story visual connectivity in the form of terraces and balconies.

Office Building.

- Tall (20'-22') ground-level storefronts and ceilings, canopies, and folding storefront walls;
- Recessed (approximately 4'-12') storefront at the building's northwest corner;
- Well-detailed canopies, articulated steel detailing at the windows, and brick coursing;
- Variation in canopy detailing and form help provide a higher level of interest as well as a clear language and hierarchy that easily locates the building's main entrances; and,
- The building's main entrance is located mid-block, directly across from the MAX stop which provides a direct visual connection between platform and interior serving to improve transit accessibility.

Therefore, these guidelines are met.

B4. Provide Stopping and Viewing Places. Provide safe, comfortable places where people can stop, view, socialize and rest. Ensure that these places do not conflict with other sidewalk uses.

B5. Make Plazas, Parks and Open Space Successful. Orient building elements such as main entries, lobbies, windows, and balconies to face public parks, plazas, and open spaces. Where provided, integrate water features and/or public art to enhance the public open space. Develop locally oriented pocket parks that incorporate amenities for nearby patrons.

B5-1. Enhance the Design of Pocket Parks. Design pocket parks in residential areas with a variety of experiences that encourage their use all year round. This guideline may be accomplished by any or all of the following:

- a. Providing some shaded places protected from the wind and rain as well as generous sunny areas that will allow its use during different times of the day and year;
- b. Providing seating, trees, grass, flowering plants, paved or textured areas and/or water features; or
- c. Providing for children's play equipment with protected soft surfaces, seating and water fountains.

Findings for B4, B5 and B5-1: The project employs a variety of public activation strategies that will add a new cultural focal point and enhance the neighborhood's public place framework. Key components of this strategy are:

- **Plaza.** The on-grade plaza will be an active open space at both the south and north entries – the SW facing plaza is located to receive south and western sunlight during most of the day; and, orienting the Plaza Building's primary entrance to face the north end of the Plaza including space for covered restaurant/café seating.
- **Mid-Block Connector.** This element provides at-grade public connectivity between SW Taylor St and SW Yamhill St. It is well-activated with retail

and work/live entries (with roll-up doors), and public amenities such as seating, passive activity eddies and water features. Eyes on the Plaza from both office and residential spaces will keep it safe throughout the day and night.

- **Materials.** A simple, well-detailed materials palette in the Plaza will create welcoming warmth while unifying with the abutting streetscape. The plant palette is selected to create seasonal interest throughout the year. The proposed water feature is intended to appeal to users of all ages through interactivity and artistic expression.
- **Building Erosion, Recessed Entries and Spill-Out.** The project employs a variety of design techniques to enhance community socialization. The Office Building main entrance faces the plaza created on SW Yamhill St by the MAX station (and lack of vehicular access). The new landscaped zone between the station and the pedestrian through zone brings the natural environment to the plaza and helps to create human scale pockets with pervasive seating opportunities. The recess of the Office Building's façade at the corner of SW 18th Ave and Yamhill St enhances the openness of the public space network. Recessed retail bays and large canopies are provided on all sides of the buildings maximizing the potential for spill-out and passive gathering.

Therefore, these guidelines are met.

B7. Integrate Barrier-Free Design. Integrate access systems for all people with the building's overall design concept.

Findings: The project is fully ADA compliant, and provides barrier-free movement and entry to all main building entries and on-site pedestrian areas.

Therefore, this guideline is met.

B2. Protect the Pedestrian. Protect the pedestrian environment from vehicular movement. Develop integrated identification, sign, and sidewalk-oriented night-lighting systems that offer safety, interest, and diversity to the pedestrian. Incorporate building equipment, mechanical exhaust routing systems, and/or service areas in a manner that does not detract from the pedestrian environment.

B6-1. Provide Outdoor Lighting at Human Scale. Provide outdoor lighting at a human scale to encourage evening pedestrian activity.

C12. Integrate Exterior Lighting. Integrate exterior lighting and its staging or structural components with the building's overall design concept. Use exterior lighting to highlight the building's architecture, being sensitive to its impacts on the skyline at night.

Findings for B2, B6-1, and C12: Protect the Pedestrian. The proposed curb extension and artful crosswalk at the corner of SW Yamhill Street and SW 17th Avenue will provide a safe pedestrian crossing and connection to the light rail station. The furnishing zone of the proposed streetscape includes a mix of trees, benches, bike racks and light poles separating pedestrians from cars. Steel and glass canopies at major building entry points and at retail storefronts will provide weather protection.

Mechanical equipment is primarily kept away from pedestrians and located on the roof or penthouse levels of the buildings and garage ventilation mechanical louvers are incorporated into the overall building design and screened by well-integrated louvers and landscape elements when they occur near grade. The office

building includes a band of ventilation louvers in the top level of the retail storefronts, keeping them above pedestrians. At the Office building, the louvers are located above the canopies and away from the pedestrian realm.

Lighting. The proposed lighting plan and fixture specifications indicate a well-integrated lighting system incorporated into the project's overall design. The exterior lighting approach within the plaza and mid-block connector illuminates buildings and landscape while hiding fixtures without throwing light back into residences above. The Office Building employs a non-intrusive lighting strategy highlighting accent features – the warm wood soffits and walls that are revealed at the recesses at the upper level patio and the wider ground floor pedestrian zones (at SW18th and Yamhill) – with up-lighting the warmth and texture of the wood, without offsite light bleed impacting the nighttime skyline.

Therefore, these guidelines are met.

B6. Develop Weather Protection. Develop integrated weather protection systems at the sidewalk-level of buildings to mitigate the effects of rain, wind, glare, shadow, reflection, and sunlight on the pedestrian environment.

Findings for B6: This guideline seeks sidewalk-level protection measures to mitigate the impact of weather on pedestrians. The project includes well-integrated all-weather protection at multiple points around the sidewalk-level of all buildings. On the full block site, canopies at retail entries, lobby entries, work/live unit entries, as well as generally along sidewalk frontages to protect pedestrians from the weather. On the half block site, the office building has a covered amenity deck and a series of connected canopies around the perimeter of the building at all pedestrian entries.

Therefore, these guidelines are met.

C1. Enhance View Opportunities. Orient windows, entrances, balconies and other building elements to surrounding points of interest and activity. Size and place new buildings to protect existing views and view corridors. Develop building façades that create visual connections to adjacent public spaces.

C11. Integrate Roofs and Use Rooftops. Integrate roof function, shape, surface materials, and colors with the building's overall design concept. Size and place rooftop mechanical equipment, penthouses, other components, and related screening elements to enhance views of the Central City's skyline, as well as views from other buildings or vantage points. Develop rooftop terraces, gardens, and associated landscaped areas to be effective stormwater management tools.

Findings for C1 and C11: All three buildings feature some upper level amenity space in the form of windows, balconies, patios, terraces and roof decks providing ample opportunities for views of surrounding points of interest, such as the West Hills, Willamette River and bridges, and the more distant Cascades Mountains. Rooftop function and mechanical is well-integrated and appropriately screened. Specific features of each building are:

Residential Tower. The Residential Tower roof terrace and unit balconies will offer river and territorial views. Rooftop amenities will have similar, yet more distant views through two-story glazing in the top band of the building. On the east side of the Residential Tower, the cementitious panel facade extends to the top of the building creating an open frame which serves to enclose the amenity

spaces and stairs/elevators, and screening mechanical equipment. A series of raised planters in an order that references the traditional layout of newspaper columns create a framework for the addition of programmed outdoor space. Located between planters and buffered by areas of green roof, a dog relief area, fire pit, outdoor fitness, and dining deck occur on the west side of the roof. Mechanical equipment will be screened and unobtrusive when viewed from the street or from a distance. A small portion of a cooling tower projecting above the roof will be set back from the building edge, and will be painted to blend in with the overall composition.

Plaza Building. The upper roof of the Plaza Building includes two simple rectangular mechanical screening enclosures (sheathed in metal siding consistent with the pallet of the project) surrounded by an extensive green roof of plantings patterned after the historic course of the Willamette River. Together, these elements create a well-unified composition. The lower roof along the Mid-block Connector serves as a deck, is accessible from Level 3 offices, and features alternating planters to create a series of outdoor rooms with direct views of the activity below. The roof of the below-grade parking garage is a landscaped, publicly accessible pedestrian plaza and mid-block connector between the two buildings.

Office Building. The office building has an amenity terrace on level 7 and a west-facing rooftop deck that is surrounded by an eco-roof. This vegetated roof serves to reduce the building's stormwater runoff and reduce the building's solar heat gain, while providing a pleasant visual experience for the terrace's occupants as well as the surrounding buildings that look down upon the roof. The building's skin design continues up and around the parapet walls that surround the roof, allowing the occupant to experience the same material (pre-cast concrete panels) that adorns the exterior skin, from within the rooftop space. The mechanical equipment on the roof is shielded from view by a screen that continues the building's overall design language.

Therefore, these guidelines are met.

C1-1. Integrate Parking. Design surface parking and parking garage exteriors to visually integrate with their surroundings. This guideline may be accomplished by any or all of the following:

- a. Designing street facing parking garages to not express the sloping floors of the interior parking;
- b. Designing the sidewalk level of parking structures to accommodate active uses, display windows, public art or other features which enhance the structure's relationship to pedestrians; or
- c. Accommodating vending booths along sidewalks adjacent to parking facilities when active ground level uses are not possible.

C7-1. Reduce the Impact of Residential Unit Garages on Pedestrians. Reduce the impact on pedestrians from cars entering and exiting residential unit garages by locating garage access on alleys, wherever possible, and active spaces on ground floors that abut streets.

Findings for C1-1 and C7-1: On-site parking for both blocks is contained within fully underground, enclosed parking garages. To disguise the use, the garage entry/exit gates and exhaust louvers are designed to integrate into the ground level façades of each building. The project does not have individual residential unit garages, but the impact of vehicles entering the block is minimized by

locating all underground parking garage access and loading access for the block in one location on the east side of the full block Residential Tower.

Special garage and loading door detailing, steel and glass canopies, and landscaping further enhance the pedestrian experience around the block. The half block building is office and retail use only with a below-grade garage accessed near mid-block on SW 17th Ave. In order to mitigate the impact of exiting vehicles on the pedestrian realm, consideration has been given to the design of the perforated garage doors to reduce headlight glare at eye-level.

Therefore, these guidelines are met.

C2. Promote Quality and Permanence in Development. Use design principles and building materials that promote quality and permanence.

C5. Design for Coherency. Integrate the different building and design elements including, but not limited to, construction materials, roofs, entrances, as well as window, door, sign, and lighting systems, to achieve a coherent composition.

B1-1. Provide Human Scale to Buildings along Walkways. Provide human scale and interest to buildings along sidewalks and walkways.

C8. Differentiate the Sidewalk-Level of Buildings. Differentiate the sidewalk-level of the building from the middle and top by using elements including, but not limited to, different exterior materials, awnings, signs, and large windows.

Findings for C2, C5, B1-1, and C8: All three buildings demonstrate restraint in material composition and well-considered detailing techniques which will result in a development exceeding these guidelines. Specific successful attributes include:

Full Block. Each of the two buildings on the Full Block is a coherent composition of its own, distinct from one another by design parti, program, massing, material palettes and concept; yet unified through ground level building volumes, active uses and pedestrian amenities. The project proposes a high quality palette of materials to provide a range of visual experiences with materials of permanence. The Plaza Building is a solid, compact monolithic composition of punched openings in brick. The brick detailing proposed for the building adds a human-scale layer of texture evident at the ground level as well as from other public vantage points. A high-volume ground level accented with exposed steel detailing, double-height glazed aluminum storefront and a steel-clad side car component (2-level work-live units) conveys quality and permanence.

The Residential Tower Building features two-level recessed bays framed in metal plate with fixed wood benches covered by glass canopies which serve to reinforce pedestrian scale while adding visual interest. As the building ascends, elevation distinction is evident – staggered balcony and window placement on the west and east elevation evoke type-face, while the pure verticality of the north and south elevations evoke sheets of newsprint. Stone, cementitious panels, steel detailing, and aluminum storefront are proposed at the base of the Residential Tower Building to differentiate the pedestrian realm, with cementitious panels and aluminum window wall to accent the levels above.

As a composition, the buildings form a coherent full-block development unified via subtle ground level treatments – elements of streetscape design, bay dimensions, and similarly detailed ground-level openings framed in steel create an overall coherency for the project without making a monotonous matching block. Both

buildings will include high quality detailing of light, small-scale steel elements such as window surrounds, railings and canopies.

Half Block. The Office Building is a restrained composition of limited materiality expressed as a partially eroded grid of vertical and horizontal precast concrete panels; honeycomb-backed metal panel infill that adds to the large window bays within the concrete framework; large fiber-glass and aluminum window systems; accent wood surrounds where the building breaks the frame, both at the ground floor and upper floor terraces. The relatively straightforward grid pattern is interrupted intermittently creating large window openings which intended to evoke a newspaper page's type-face interspersed with images. The result is a rhythmic clarity and coherence that can be experienced from a pedestrian scale, as well as from distance in a larger urban context. The deeply recessed windows (approximately 10") break the overall scale of the building, while the addition of flush honeycomb-backed metal panels above and below the windows provide an additional high-quality and permanent material that adds to the range of visual interest of the overall façade.

Therefore, these guidelines are met.

II. MODIFICATION REQUESTS (33.825)

33.825.040 Modifications That Will Better Meet Design Review Requirements:

The review body may consider modification of site-related development standards, including the sign standards of Chapters 32.32 and 32.34 of the Sign Code, as part of the design review process. These modifications are done as part of design review and are not required to go through the adjustment process. Adjustments to use-related development standards (such as floor area ratios, intensity of use, size of the use, number of units, or concentration of uses) are required to go through the adjustment process. Modifications that are denied through design review may be requested as an adjustment through the adjustment process. The review body will approve requested modifications if it finds that the applicant has shown that the following approval criteria are met:

- A. Better meets design guidelines.** The resulting development will better meet the applicable design guidelines; and
- B. Purpose of the standard.** On balance, the proposal will be consistent with the purpose of the standard for which a modification is requested.

The following three modifications are requested: (1) Ground Floor Windows in the CX Zone (33.140.230); (2) Ground Floor Active Uses (33.510.225); and, (3) Bike Parking (33.266.220).

1. Ground Floor Windows (33.140.230). Purpose: Blank walls on the ground level of buildings are limited in order to:

- Provide a pleasant, rich, and diverse pedestrian experience by connecting activities occurring within a structure to adjacent sidewalk areas;
- Encourage continuity of retail and service uses;
- Encourage surveillance opportunities by restricting fortress-like facades at street level; and
- Avoid a monotonous pedestrian environment.
- The plan district modifications to the base zone standards for ground floor windows are intended to promote ground floor windows in a larger number of situations than in the base zones and to provide additional flexibility in meeting the standard.

In the CX zone, all exterior walls on the ground level which face a street lot line, sidewalk, plaza, or other public open space or right-of-way must have windows. The windows must be at least 50 percent of the length and 25 percent of the ground level wall area.

2. Ground Floor Active Uses (33.510.225). Purpose: The ground floor active use standards are intended to reinforce the continuity of pedestrian-active ground-level building uses. The standards are also to help maintain a healthy urban district through the interrelationship of ground-floor building occupancy and street level accessible public uses and activities. Active uses include but are not limited to: lobbies, retail, residential, commercial, and office.

The proposed buildings meet or exceed both of these standards, with the singular exception of the 100 foot long East elevation (SW 17th Ave) of the Office Building. Regarding Ground Floor Windows, the proposal does not meet the required 50% of length standard – approximately 44’ of window length for 100’ of wall length. Therefore, the proposal is approximately 6’ (6%) shy of the glazing length requirement. Regarding Ground Floor Active Use, the proposal does meet the required 50% of length standard – approximately 39.5’ of active use length for 100’ of wall length.

Findings for these two Modifications have been combined due to the similarity in purpose statements – primarily intending ground level activity, continuity of public access and diversity of pedestrian experience.

A. Better meets design guidelines. *The resulting development will better meet the applicable design guidelines.*

Findings. Of the project’s eleven elevations fronting public space or pedestrian areas, 10 exceed the ground floor window standards. The SW 17th Ave elevation (east) of the Office Building falls short of the 50% of ground floor window length requirement by approximately 6% (or approximately 6’ of the 100’ frontage). Compliant conditions on this elevation are glazing at retail and pedestrian circulation, while non-compliant conditions are limited to loading and parking garage doors, louver and structural columns. Given the lack of “blank wall” conditions on this elevation, there exists no opportunity to apply mitigation (in the form of art, seating, signage, etc). Also, given that light-rail fronts the building’s other two street-facing elevations, the SW 17th Ave elevation is the only location for parking/loading access. Therefore, the approval nexus for this Modification must consider the development as a whole – design guidelines better met by the design as proposed for all ground level building faces are described below.

A4 Use Unifying Elements: All elevations are treated as public faces for the development featuring a wide array urban design and architectural elements that serve to unify project and place – active ground-level floor area, spill-out areas, high-volume storefront glazing, street-edge seating and planting, historic light standards, artistic crosswalks, storefront detailing, etc.

A5-2 Strengthen the Identity of the Civic Stadium Station Area: By necessity, the only location for the garage and loading entry is the SW 17th Ave elevation – all other elevations are fronted by light-rail alignment and platform. Preserving the integrity and viability of this transit supportive public space is fundamental to strengthening community identity.

B1-1 Provide Human Scale to Buildings Along Walkways: High-volume ground level spaces and large expanses of clear glazing along all elevations will allow

views into active spaces. Sensitive detailing of transitions between public and private space including low planter walls, high and low plantings, trellises and canopies, storefront detailing, etc provide human scale.

B5 Make Plazas, Parks and Open Space Successful: Locating service entries to the south end of the SW 17th Avenue away from the MAX platform is critical to the success of this community asset. This allows the platform to maintain safe operations while maximizing spill out potential from the abutting office building. In addition, locating the garage/loading entry on SW 17th Ave relieves the pressure to serve the project more proximate to the project’s other public space amenities – the mid-block connector and the plaza.

Therefore this criterion is met.

B. Purpose of the standard. *On balance, the proposal will be consistent with the purpose of the standard for which a modification is requested.*

Findings. As mentioned above, the purpose statements for these two standards are similar in that they seek to limit blank walls on the ground level of buildings in order to: provide a pleasant, rich, and diverse pedestrian experience by connecting activities occurring within a structure to adjacent sidewalk areas, or allowing public art at the ground level; encourage continuity of retail and service uses; encourage surveillance opportunities by restricting fortress-like facades at street level; avoid a monotonous pedestrian environment; and, maintain a healthy urban district through the interrelationship of ground-floor building occupancy and street level accessible public uses and activities

On balance, this project meets the purpose of this standard by visually and physically connecting activities within the structures to adjacent sidewalk areas and public spaces. With 11 public elevations to the project, there is no ideal location for garage entry ramps, loading bays or trash/recycling. However, due to the light-rail alignment on SW 18th Ave and Yamhill St and the desire to keep the southwest facing plaza pedestrian-oriented, the logical location for loading and garage access is on SW 17th Ave.

Locating service and garage access on SW 17th Ave incrementally reduces glazing and active floor area and on this elevation by 6’ and 10’ respectively. Specific features meeting the purpose of this standard are:

- glazed views into the active floor area fronting the MAX platform;
- high quality, well-integrated, pedestrian-friendly wall systems – garage doors, louvers, storefront;
- varied landscape plantings along all building elevations;
- visual and physical connection through development at the mid-block connector; and,
- pervasive activity generated by active floor area, spill-out activity, and exterior public space programming and amenities (art, seating, water feature, etc.)

This criterion is met.

Therefore, this Modification merits approval.

3. Bike Parking Space Dimension (33.266.220.C.3.b.) A space 2 feet by 6 feet must be provided for each required bicycle parking space, so that a bicycle six feet long can be securely held with its frame supported so that the bicycle cannot be pushed or fall in a manner that will damage the wheels or components. The project proposes staggered vertical storage, dimensioned at 18” x 6’ with a 5’ walk aisle.

A. Better meets design guidelines. *The resulting development will better meet the applicable design guidelines.*

Findings: The project includes approximately 700 total long term bicycle parking spaces which exceed the code requirement by approximately 170 spaces. Accommodating the bicycle parking spaces within a 24” width module would consume considerably more floor area. Relying upon a staggered 18” wide vertical/wall hanging bike rack system is a far more efficient use of space and is identical to the parking system conventionally approved in the Central City. The proposed functional and space efficient system better meets design guidelines because it eases programmatic demand thereby allowing more active uses at the street – *A8 Contribute to a Vibrant Streetscape, C9 Develop Flexible Sidewalk-Level Spaces. This criterion is met.*

B. Purpose of the standard. *On balance, the proposal will be consistent with the purpose of the standard for which a modification is requested.*

Purpose: These standards ensure that required bicycle parking is designed so that bicycles may be securely locked without undue inconvenience and will be reasonably safeguarded from intentional or accidental damage.

Findings: The proposed Bike Rack system is engineered to stagger bikes vertically to allow the handle bars to overlap. This allows the racks to provide the same level of service that would be provided by a standard 24” on center spacing within an 18” space. The staggered clearance between adjacent bikes and allowance for sliding hangers eases the hanging and locking of a bike. Additionally the loops, to which the bikes are locked, project out of the wall approximately 18” to further ease hanging and locking bikes. A 5’ minimum aisle is still provided behind each bicycle rack. The rack system will be located within secure bike storage enclosures below grade with lobby and bike lounge access. The bicycle parking system is safe and secure, located in a convenient area, and is designed to avoid initial or accidental damage to bicycles, so the proposal is consistent with the purpose statement of the bicycle parking standards. *This criterion is met.*

This Modification therefore warrants approval.

III. ADJUSTMENT REQUESTS (33.805)

33.805.010 Purpose

The regulations of the zoning code are designed to implement the goals and policies of the Comprehensive Plan. These regulations apply city-wide, but because of the city's diversity, some sites are difficult to develop in compliance with the regulations. The adjustment review process provides a mechanism by which the regulations in the zoning code may be modified if the proposed development continues to meet the intended purpose of those regulations. Adjustments may also be used when strict application of the zoning code's regulations would preclude all use of a site. Adjustment reviews provide flexibility for unusual situations and allow for alternative ways to meet the purposes of the code, while allowing the zoning code to continue to provide certainty and rapid processing for land use applications.

33.805.040 Approval Criteria

All adjustment requests will be approved if the review body finds that the applicant has shown that approval criteria A. through F. have been met.

- A. *Granting the adjustment will equally or better meet the purpose of the regulation to be modified; and*
- B. *If in a residential zone, the proposal will not significantly detract from the livability or appearance of the residential area, or if in an OS, C, E, or I zone, the proposal will be consistent with the classifications of the adjacent streets and the desired character of the area; and*
- C. *If more than one adjustment is being requested, the cumulative effect of the adjustments results in a project which is still consistent with the overall purpose of the zone; and*
- D. *City-designated scenic resources and historic resources are preserved; and*
- E. *Any impacts resulting from the adjustment are mitigated to the extent practical; and*
- F. *If in an environmental zone, the proposal has as few significant detrimental environmental impacts on the resource and resource values as is practicable; or*

The following adjustment is requested:

1. Quantity of Loading Spaces (33.266.310.C.) Two on-site loading spaces meeting Standard A are required for the Office Building on the half block. The project proposes one Standard A loading space on site abutting SW 17th Ave.

- A. *Granting the adjustment will equally or better meet the purpose of the regulation to be modified; and*

Purpose Statement: *A minimum number of loading spaces are required to ensure adequate areas for loading for larger uses and developments. These regulations ensure that the appearance of loading areas will be consistent with that of parking areas. The regulations ensure that access to and from loading facilities will not have a negative effect on the traffic safety or other transportation functions of the abutting right-of-way.*

Findings: The Applicant has submitted a Loading and Queuing Analysis (Exhibit A.5) as required by PBOT demonstrating that the proposed loading configuration will sufficiently and safely serve the site, and will not have a negative effect on the traffic safety or other transportation functions of the abutting right-of-way. *Therefore, this criterion is met.*

- B. *If in a residential zone, the proposal will not significantly detract from the livability or appearance of the residential area, or if in a C, E, or I zone, the proposal will be consistent with the desired character of the area; and*

Findings: The site is located in Portland’s highest density commercial zone, CX. The portion of the garage expressed on the exterior is limited to the 14’ wide loading garage door adjacent to a 22’ wide parking garage door. Each contains a high-speed perforated coiling door to obscure the interior vehicles activities and mitigate light glare at pedestrian eye-level. The remaining of the ground floor contains pedestrian-scaled details like canopies, extensive glazing, light fixtures and materials detailing. Limiting the visibility of the parking and featuring such design elements will further enhance the livability of the Goose Hollow neighborhood. *This criterion is therefore met.*

- C. *If more than one adjustment is being requested, the cumulative effect of the adjustments results in a project which is still consistent with the overall purpose of the zone; and*

Findings: Only one Adjustment is requested. *This criterion does not apply.*

D. *City-designated scenic resources and historic resources are preserved;* and

Findings: There are no city-designated scenic or historic resources on this site. *This criterion does not apply.*

E. *Any impacts resulting from the adjustment are mitigated to the extent practical;* and

Findings: As mentioned in the findings above, the vehicle activities will be screened by a high-speed perforated coiling garage door. In addition, the reduction in the number of required loading spaces reduces the negative effects of an additional curbcut and blank wall surfaces and frees up the square footage to provide areas for more active leasable uses such as retail. The Applicant has submitted a Loading and Queuing Analysis as required by Portland Bureau of Transportation demonstrating that the impacts to traffic operations are mitigated to the extent practical. *Therefore, this criterion is met.*

F. *If in an environmental zone, the proposal has a few significant detrimental environmental impacts on the resource and resource values as is practicable;*

Findings: This site is not within an environmental zone. *This criterion does not apply.*

Therefore, approval of this Adjustment is warranted.

DEVELOPMENT STANDARDS

Unless specifically required in the approval criteria listed above, this proposal does not have to meet the development standards in order to be approved during this review process. The plans submitted for a building or zoning permit must demonstrate that all development standards of Title 33 can be met, or have received an Adjustment or Modification via a land use review prior to the approval of a building or zoning permit.

CONCLUSIONS

The design review process exists to promote the conservation, enhancement, and continued vitality of areas of the City with special scenic, architectural, or cultural value. The applicant has addressed all major concerns expressed by staff and designed a building that embodies the spirit intended by the applicable design guidelines, and responds well to the natural, cultural and built context. The proposal meets, and in some cases exceeds the applicable design guidelines, and Modification and Adjustment criteria and therefore warrants approval.

TENTATIVE STAFF RECOMMENDATION

(May be revised upon receipt of new information at any time to the Design Commission decision)

Staff recommends approval of a 3-building development on two parcels under common ownership on one and a half blocks in the Central City – Goose Hollow Plan District identified as follows

- **Full Block** – 1621 SW Taylor St, bound by SW Taylor St (south), SW Yamhill St (north), SW 16th Ave (east) and SW 17th Ave (west). Two buildings: the **Residential Tower Building** – approximately 344,000 gross square feet, 250' in height, including 337 residential units of varying sizes, three (3) additional work/live units reside along the Mid-block Connector; and, the **Plaza Building** – approximately 41,500 gross square feet, 55' in height, including approximately 9,000 net square feet of retail space and two levels of office space above (totaling approximately 22,000 gross square feet.)
- **Half Block** – 817 SW 17th Ave, bound by SW Yamhill St (north), SW 17th Ave (east) and SW 18th Ave (west). The **Office Building** – approximately 152,349 gross square feet, 8-story, 136' in height, mixed-use office building.

Staff recommends approval of the following Modifications (3), Adjustment and Design Exception:

Modifications. Three (3) Modifications are requested:

1. **Ground Floor Windows in the CX Zone (33.130.230).** Required – 50% of ground floor wall length must contain windows with views into working areas. Proposed – the east elevation of the half block building proposes windows on approximately 43'-9" (approximately 44%) of the ground floor length.
2. **Ground Floor Active Use (33.510.225).** Required – 50% of the ground floor wall length must contain active uses. Proposed – the east elevation of the half block building proposes active use for 39'-6" (approximately 40%) of the ground floor wall.
3. **Bike Parking Stall Width (33.266.220.C3).** Required – a space 24" x 6' must be provided for each required bicycle parking space. Proposed – 18" spacing for hung racks with a 6" vertical stagger.

Adjustments. One (1) Adjustment is requested:

- **Quantity of Loading Spaces (33.266.310.C.)** Two on-site loading spaces meeting Standard A are required for the project. The project proposes one Standard A loading space on site – adjacent to the drive aisle opposite the at-grade mechanical parking. *Loading/Queuing Analysis approved by PBOT.*

Design Exception. One (1) Design Exception (DE) is requested:

- **Garage Door Setback** – 20' required, 3' proposed. *DE approved by PBOT.*

If approved, staff recommends the following conditions:

- A. As part of the building permit application submittal, the following development-related conditions (B through E) must be noted on each of the 4 required site plans or included as a sheet in the numbered set of plans. The sheet on which this information appears must be labeled "ZONING COMPLIANCE PAGE - Case File LU 16-283891 DZ." All requirements must be graphically represented on the site plan, landscape, or other required plan and must be labeled "REQUIRED."
- B. At the time of building permit submittal, a signed Certificate of Compliance form (<https://www.portlandoregon.gov/bds/article/623658>) must be submitted to ensure the permit plans comply with the Design/Historic Resource Review decision and approved exhibits.
- C. No field changes allowed.
- D. The Central City Parking Review shall be approved prior to the issuance of any building permits.
- E. Should the Central City Parking Review require changes to the design of the proposal, a follow-up Type II LUR shall be required.

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Procedural Information. The application for this land use review was submitted on November 14, 2016, and was determined to be complete on December 6, 2016.

Zoning Code Section 33.700.080 states that Land Use Review applications are reviewed under the regulations in effect at the time the application was submitted, provided that the application is complete at the time of submittal, or complete within 180 days. Therefore this application was reviewed against the Zoning Code in effect on November 14, 2016.

ORS 227.178 states the City must issue a final decision on Land Use Review applications within 120-days of the application being deemed complete. The 120-day review period may be waived or extended at the request of the applicant. In this case, the applicant extended the 120-day review period by 30 days (Exhibit A2). Unless further extended by the applicant, **the 120 days will expire on June 4, 2017.**

Some of the information contained in this report was provided by the applicant. As required by Section 33.800.060 of the Portland Zoning Code, the burden of proof is on the applicant to show that the approval criteria are met. The Bureau of Development Services has independently reviewed the information submitted by the applicant and has included this information only where the Bureau of Development Services has determined the information satisfactorily demonstrates compliance with the applicable approval criteria. This report is the recommendation of the Bureau of Development Services with input from other City and public agencies.

This report is not a decision. The review body for this proposal is the Design Commission who will make the decision on this case. This report is a recommendation to the Design Commission by the Bureau of Development Services. The review body may adopt, modify, or reject this recommendation. The Design Commission will make a decision about this proposal at the hearing or will grant a continuance. Your comments to the Design Commission can be mailed, c/o the Design Commission, 1900 SW Fourth Ave., Suite 5000, Portland, OR 97201 or faxed to 503-823-5630.

You will receive mailed notice of the decision if you write a letter received before the hearing or testify at the hearing, or if you are the property owner or applicant. You may review the file on this case by appointment at our office at 1900 SW Fourth Ave., Suite 5000, Portland, OR 97201. Please call the file review line at 503-823-7617 to schedule an appointment.

Appeal of the decision. The decision of the Design Commission may be appealed to City Council, who will hold a public hearing. If you or anyone else appeals the decision of the review body, only evidence previously presented to the review body will be considered by the City Council.

Who can appeal: You may appeal the decision only if you write a letter which is received before the close of the record for the hearing, if you testify at the hearing, or if you are the property owner/applicant. **Appeals must be filed within 14 days of the decision. An appeal fee of \$5,000.00 will be charged.**

Additional information on how to file and the deadline for filing an appeal will be included with the decision. Assistance in filing the appeal and information on fee waivers are available from the Bureau of Development Services in the Development Services Center, 1900 SW Fourth Ave., First Floor. Neighborhood associations

recognized by the Office of Neighborhood Involvement may qualify for a waiver of the appeal fee provided that the association has standing to appeal. The appeal must contain the signature of the Chair person or other person authorized by the association, confirming the vote to appeal was done in accordance with the organization's bylaws.

Neighborhood associations, who wish to qualify for a fee waiver, must complete the Type III Appeal Fee Waiver Request for Organizations Form and submit it prior to the appeal deadline. The Type III Appeal Fee Waiver Request for Organizations Form contains instructions on how to apply for a fee waiver, including the required vote to appeal.

Recording the final decision.

If this Land Use Review is approved the final decision must be recorded with the Multnomah County Recorder. A few days prior to the last day to appeal, the City will mail instructions to the applicant for recording the documents associated with their final land use decision.

- A building or zoning permit will be issued only after the final decision is recorded.

The applicant, builder, or a representative may record the final decision as follows:

- **By Mail:** Send the two recording sheets (sent in separate mailing) and the final Land Use Review decision with a check made payable to the Multnomah County Recorder to: Multnomah County Recorder, P.O. Box 5007, Portland OR 97208. The recording fee is identified on the recording sheet. Please include a self-addressed, stamped envelope.
- **In Person:** Bring the two recording sheets (sent in separate mailing) and the final Land Use Review decision with a check made payable to the Multnomah County Recorder to the County Recorder's office located at 501 SE Hawthorne Boulevard, #158, Portland OR 97214. The recording fee is identified on the recording sheet.

For further information on recording, please call the County Recorder at 503-988-3034. For further information on your recording documents please call the Bureau of Development Services Land Use Services Division at 503-823-0625.

Expiration of this approval. An approval expires three years from the date the final decision is rendered unless a building permit has been issued, or the approved activity has begun.

Where a site has received approval for multiple developments, and a building permit is not issued for all of the approved development within three years of the date of the final decision, a new land use review will be required before a permit will be issued for the remaining development, subject to the Zoning Code in effect at that time.

Zone Change and Comprehensive Plan Map Amendment approvals do not expire.

Applying for your permits. A building permit, occupancy permit, or development permit must be obtained before carrying out this project. At the time they apply for a permit, permittees must demonstrate compliance with:

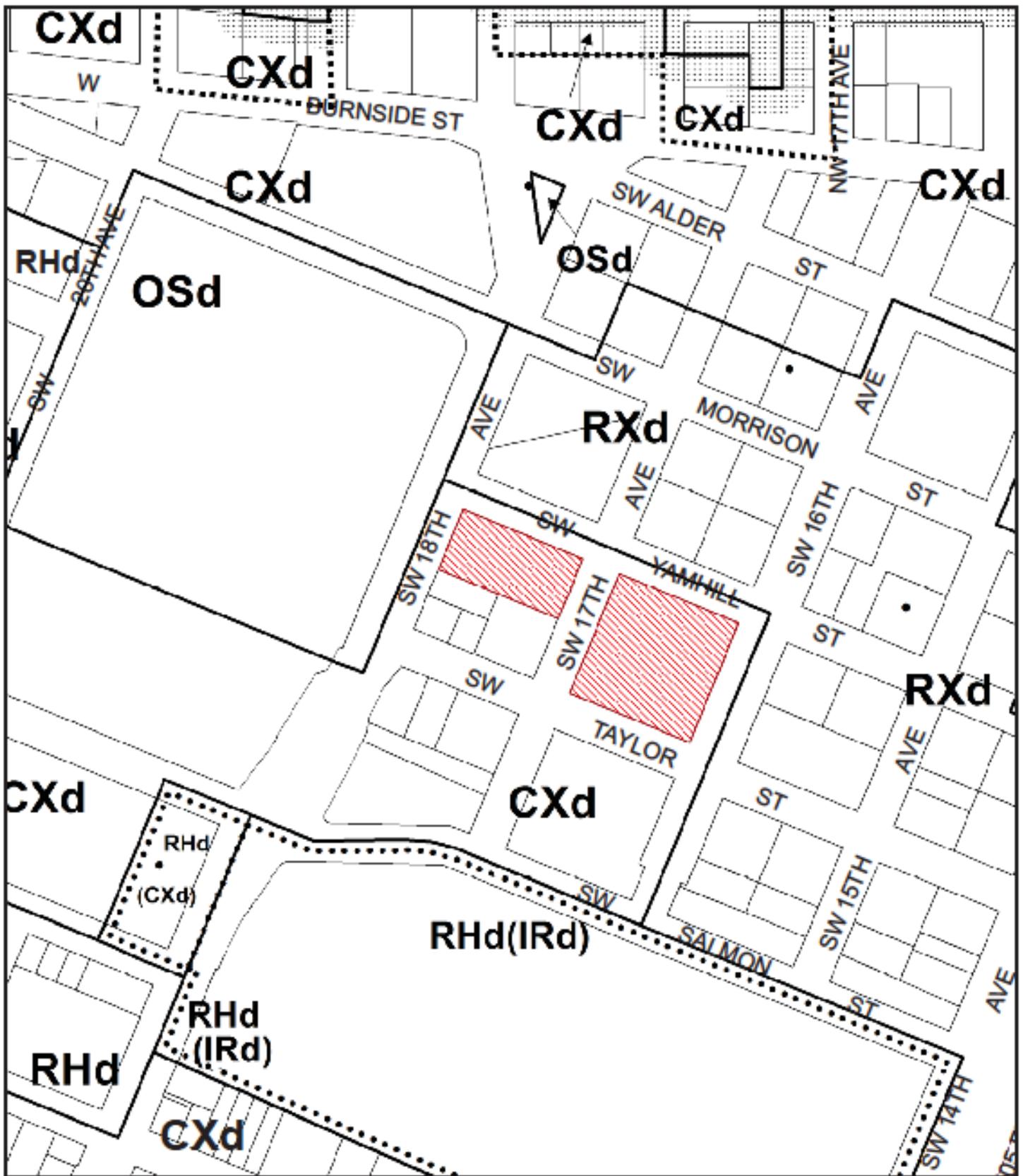
- All conditions imposed here.
- All applicable development standards, unless specifically exempted as part of this land use review.
- All requirements of the building code.
- All provisions of the Municipal Code of the City of Portland, and all other applicable ordinances, provisions and regulations of the city.

The Bureau of Development Services is committed to providing equal access to information and hearings. Please notify us no less than five business days prior to the event if you need special accommodations. Call 503-823-7300 (TTY 503-823-6868).

Jeffrey Mitchem
February 21, 2017

EXHIBITS – NOT ATTACHED UNLESS INDICATED

- A. Applicant’s Statement
 - 1. Narrative
 - 2. 120-Day Extension
 - 3. LUR Application Drawing Set
 - 4. LUR Hearing 1 Revised Drawing Set
 - 5. Loading and Queuing Analysis
 - 6. Manufacture’s Cutsheets
- B. Zoning Map (attached)
- C. Plan & Drawings
 - 1. Design Review Drawing Set (Sheet C.1-C.173)
 - Sheet C.4 Site Plan (attached)
 - Sheet C.5 North Elevation (attached)
 - Sheet C.6 South Elevation (attached)
 - Sheet C.27 East Elevation (attached)
 - Sheet C.28 West Elevation (attached)
- D. Notification information:
 - 1. Request for response
 - 2. Posting letter sent to applicant
 - 3. Notice to be posted
 - 4. Applicant’s statement certifying posting
 - 5. Mailed notice
 - 6. Mailing list
- E. Agency Responses:
 - 1. Site Development
 - 2. Fire Bureau
 - 3. Bureau of Environmental Services
 - 4. Life Safety
 - 5. Water Bureau
 - 6. Portland Bureau of Transportation
- F. Letters:
 - 1. Tracy Prince, email dated December 14, 2017. Comments in opposition to the project citing ground floor windows and ground floor active use Modifications.
 - 2. Michael Mehaffy, President, Goose Hollow Foothills League. Comments in opposition to the project citing height, mass, scale, shadow and context.
- G. Other
 - 1. Original LUR Application
 - 2. DAR Summary Memorandum, September 30, 2016
 - 3. DAR Summary Memorandum, November 10, 2016
- H. Post First Hearing



ZONING



This site lies within the:
CENTRAL CITY PLAN DISTRICT
GOOSE HOLLOW Sub Area

- Site
- Historic Landmark

File No.	LU16-273094 DZM, AD
1/4 Section	3028
Scale	1 inch = 200 feet
State_Id	1N1E33DC 3600
Exhibit	B (Mar 3, 2017)



- COMMERCIAL
- LIVE/WORK
- OFFICE
- RESIDENTIAL AMENITY
- BIH / SERVICES
- PARKING

- MAIN BUILDING ENTRY
- COMMERCIAL ENTRANCE
- WORK/LIVE ENTRANCE
- CONTROLLED ACCESS
- GARAGE ENTRY
- LOADING ENTRY
- BIKE ROOM ACCESS
- ON-SITE LOADING
- TRI - MET STATION



MASTER SITE | USE AND ACCESS PLAN

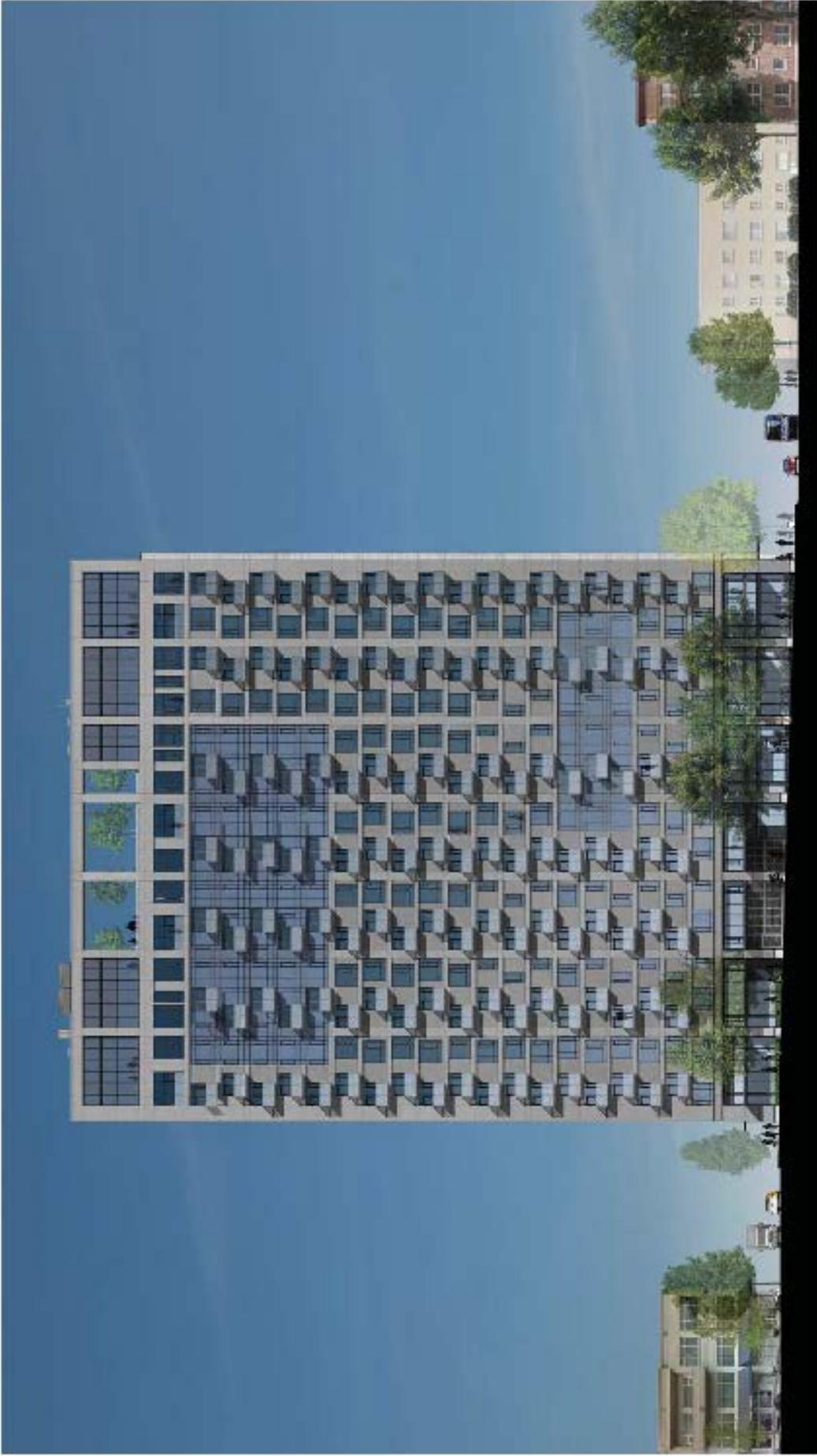


MASTER SITE | NORTH ELEVATION

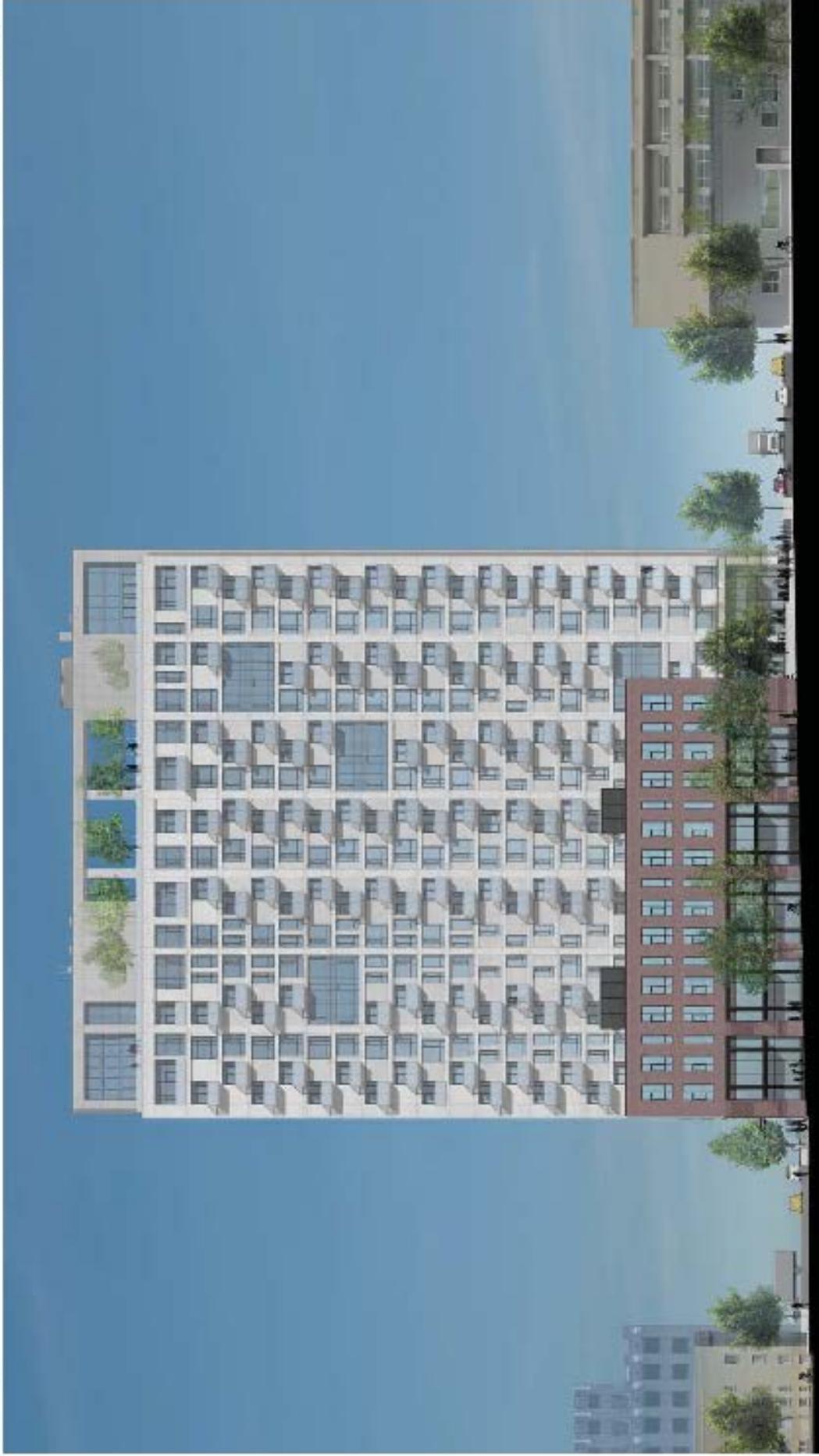




MASTER SITE | SOUTH ELEVATION



FULL BLOCK | EAST ELEVATION



FULL BLOCK | WEST ELEVATION