



**NOTICE OF FINAL
 FINDINGS, CONCLUSIONS AND DECISION
 OF THE CITY OF PORTLAND DESIGN COMMISSION
 ON AN
 APPEALED ADMINISTRATIVE DECISION
 (Type II Process)**

**CASE FILE: LU 17-113306 DZM – CENTRAL LOFTS MIXED USE
 LOCATION: 8608 North Lombard Street**

The administrative decision for this case, published on October 18, 2017 was appealed to the Design Commission by the St. Johns Neighborhood Association.

A public hearing was held December 14, 2017. At the hearing, the Design Commission voted four to zero to deny the appeal, and uphold the Staff Decision. The original analysis, findings and conclusion have been revised by the Design Commission as follows. This decision is available on line: <http://www.portlandonline.com/bds/index.cfm?c=46429>

GENERAL INFORMATION

- Architect:** Alan Jones & Sienna Shiga | Jones Architecture
120 NW 9th Ave., Suite 210 | Portland, OR 97209
- Owner:** St John-3 LLC
10260 SW Greenburg Road #1180 | Portland, OR 97223
- Site Address:** **8608 N Lombard Street**
- Legal Description:** BLOCK 39 SELY 1/2 OF LOT 4 EXC PT IN ALLEY, JAMES JOHNS ADD
- Tax Account No.:** R425804370
- State ID No.:** 1N1W12BA 06600
- Quarter Section:** 2121
- Neighborhood:** St. Johns, contact Rachel Hill at hill.rachel@gmail.com
- Business District:** St. Johns Center for Opportunity, contact Emily Stanfield at 503-841-5522.
- District Coalition:** North Portland Neighborhood Services, contact Mary Jaron Kelley at 503-823-4099.
- Plan District:** St. Johns
- Zoning:** **CSd** – *Storefront Commercial with Design Overlay*
- Case Type:** **DZM** – *Design Review w/ Modifications*
- Procedure:** **Type II**, an administrative decision, appealed to the Design Commission.

Proposal:

The applicant seeks Design Review for a four-story building fronting the public plaza at the intersection of N. Lombard and N. Philadelphia streets. The ground floor will be a leasable commercial space, designed to accommodate a restaurant. The upper three floors will be residential units. The residential lobby and long-term bike parking will be accessed from the south end of the N. Philadelphia frontage. One Modification to reduce long-term bike parking spacing from 24” to 18” is requested. One Design Exception to reduce the spacing between oriel projections, from 12’ to 6’ on the North Lombard frontage is requested.

Relevant Approval Criteria:

In order to be approved, this proposal must comply with the approval criteria of Title 33, Portland’s Zoning Code. The relevant approval criteria are:

- *Community Design Guidelines*
- *Modifications That Will Better Meet Design Review Requirements – 33.825.040*

ANALYSIS

Site and Vicinity: The 6900-square foot site is located on the west side of N. Lombard Street, at the intersection of N. Philadelphia Avenue and N. Lombard Street, and across from N. Burlington Avenue. To the south and east of the site, a deep public plaza separates it from its adjacent street frontages. The plaza includes low seating walls, stepped areas, and mature trees. It is the heart of the neighborhood's programmed civic activity, hosting the weekly Farmer's Market in the summer, outdoor concerts and festivals. It is the heart of the "Main Street" commercial corridor of the St. Johns neighborhood.

North Lombard is characterized by storefront commercial buildings constructed over several decades interspersed with utilitarian service buildings. With a few exceptions, buildings are a maximum of two stories, and primarily brick or stucco.

The site abuts one building, a narrow one-story Thai restaurant to the northwest. Behind the site, a narrow mid-block alley separates it from the half block site to the SW. That site is occupied by a one-story Burgerville and surface parking. Across N. Philadelphia street to the southeast is the US Bank, flanked by surface parking to the north and south.

Zoning: The Storefront Commercial (CS) zone is intended to preserve and enhance older commercial areas that have a storefront character. The zone intends that new development in these areas will be compatible with this desired character. The zone allows a full range of retail, service and business uses with a local and regional market area. Industrial uses are allowed but are limited in size to avoid adverse effects different in kind or amount than commercial uses and to ensure that they do not dominate the character of the commercial area. The desired character includes areas which are predominately built-up, with buildings close to and oriented towards the sidewalk especially at corners. Development is intended to be pedestrian-oriented and buildings with a storefront character are encouraged.

The "d" overlay promotes the conservation and enhancement of areas of the City with special historic, architectural or cultural value. New development and exterior modifications to existing development are subject to design review. This is achieved through the creation of design districts and applying the Design Overlay Zone as part of community planning projects, development of design guidelines for each district, and by requiring design review. In addition, design review ensures that certain types of infill development will be compatible with the neighborhood and enhance the area.

Land Use History: City records indicate no prior land use reviews.

Agency Review: A "Notice of Proposal in Your Neighborhood" was mailed **August 23, 2017**. The following Bureaus have responded with no issues or concerns:

- Bureau of Environmental Services. Please see Exhibit E-1 for additional details.
- Portland Bureau of Transportation. Please see Exhibit E-2 for additional details.
- Water Bureau. Please see Exhibit E-3 for additional details.
- Fire Bureau. Please see Exhibit E-4 for additional details.
- Site Development Section of BDS. Please see Exhibit E-5 for additional details.

Neighborhood Review: A Notice of Proposal in Your Neighborhood was mailed on August 23, 2017. A total of four written responses have been received from either the Neighborhood Association or notified property owners in response to the proposal.

- **Joshua Guerra**, July 12, 2017. *Wrote with concerns about the proposal's response to surrounding contextual architecture. Suggested: brick or stucco cladding; stronger relationship with the plaza; utilize pedestrian-scale elements at the street level. Mr. Guerra also sent a .pdf presentation that he presented at the St. Johns Neighborhood Association meeting. Please see Exhibit F.1 for more detail.*
- **Christa Holden**, July 14, 2017. *Wrote with concerns that the residential and commercial entries were not obvious. The ground level is not distinguished from the upper floors. She suggested more operable openings to tie the ground floor to the plaza, or balconies on the upper floors. Please see Exhibit F.2 for more detail.*

- **Holly Hagerman**, September 23, 2017. *Feels the exterior design does not fit with the aesthetics of main street. Please see Exhibit F.3 for more detail.*

Staff Response to the above comments: *The proposal that public comments were reacting to has been significantly modified to better achieve the contextual responses. Large, operable glazing panels were located at the corner to allow the ground floor to open to the plaza. Additional commercial entrances were located off the plaza, and the primary entrances to the commercial and residential spaces were moved to the far corners closest to street frontages, to facilitate visibility. Pedestrian-scale, contextual elements were added, including canopies, wood cladding at the recessed entries, integrated lighting and mounts to accommodate future signage. While the primary cladding material is not stucco or brick, the terra-cotta color does reference the red brick of the US Bank across N. Philadelphia. The smooth, flat cementitious panels create large, continuous planes that are visually similar to stucco.*

- **St. Johns Neighborhood Association**, September 23, 2017. *A stakeholder group, including the St. Johns Neighborhood Association, St. Johns Center for Opportunity, and the St. Johns Boosters wrote with concerns. Suggestions include: suggestions that the Title 33 code requirements for Ground Floor Windows should also apply to the N. Philadelphia façade as well as the N. Lombard façade; reduced height would be more contextually responsive; singular massing is overwhelming to pedestrians; reduced width of windows; utilize brick, stucco or horizontal wood; locate primary entrance at the plaza corner; treat the site as a gateway; enhance the pedestrian realm with more pedestrian-scale treatment; reduce the scale of upper story windows; add a parapet and more detailing. Please see Exhibit F.4 for more detail.*

Staff Response to the Neighborhood Association: *The Ground Floor Window standards (33.130. 230.B1) specifically applies only on the frontage that has the highest transit street classification. Despite the significance of both street frontages, Staff cannot apply code standards to areas or frontages where they do not apply. However, in response to the comments, the applicant voluntarily chose to increase the ground floor windows on the N. Philadelphia frontage. The result is that both street frontages exceed the 50% of the façade length and 25% of the ground level wall area that is required of only one frontage.*

Similarly, the proposal complies with the zoning code height limitations, and does not request any height Modifications. Site-specific height maximums are determined by the Bureau of Planning and Sustainability (BPS) through determination of zoning and overlays. Bureau of Development Services staff enforce Title 33, Planning and Zoning code, but do not have the ability to require a height reduction or other change to development standards. If the community feels that the maximum heights for this area are excessive, Staff recommend working with the Bureau recommends providing commentary to BPS.

Staff weighed the context of surrounding buildings, and the deep plaza wrapping both street frontages, when evaluating the appropriate architectural response. Ultimately, a survey of other buildings demonstrated that large fields of singular, undetailed material is common in the area. North Lombard is notable for its plain, utilitarian, stucco buildings, with very few historic decorative details. Staff determined that large fields of singular materials and simple, rhythmic facades are typical of the area, and that a more dramatic application of these principles is appropriate at the iconic, civic plaza location.

In response to the much smaller scale, pedestrian-oriented main street context, Staff echoed the neighborhood requests for a richer treatment of the street level. In response to the comments, the applicant added significant detail at the street level including deep canopies, operable windows at the plaza corner, horizontal wood cladding in the recessed entries, wood soffits beneath the vertical oriel bays, integrated lighting and traditional mounts to accommodate future signage.

Finally, Staff shares the public concerns about the need for high-quality, durable materials to ensure an attractive and long-lasting façade. Staff has evaluated a material sample for the proposed integral-color Cembrit cementitious panels. Staff is satisfied that this material is not susceptible to scratches, dents or streaking. Its integral color, factory-cut and pre-drilled panel system will minimize site-installation errors and will facilitate the applicant's intended smooth façade.

Procedural History:

The October 18, 2017 administrative Decision approved the proposal. The case was appealed by the St. Johns Neighborhood Association on November 1, 2017, based on Guidelines P1: *Plan Area Character*, D2: *Main Entrances* and D7: *Blending into the Neighborhood*.

At the December 14, 2017 appeal hearing with the Design Commission, Commissioners heard testimony from neighbors in support and opposition to the proposal. Commissioners discussed the appropriate contextual response and leaned heavily on Guideline P1, subsection B which describes anticipated architectural responses on commercial streets in St. Johns. This guideline says: *“Along commercial streets: Incorporating desired architectural and site development features to complement, rather than imitate, existing historic buildings. The existing storefronts illustrate some of the desired characteristics. Other desired site features include outdoor space for dining and other activities, and building frontage and setbacks integrated with the public realm.”* The Commission determined that this neighborhood-specific guideline had been met.

The Design Commission voted unanimously to uphold the Staff Decision and deny the appeal.

ZONING CODE APPROVAL CRITERIA

Chapter 33.825 Design Review

Section 33.825.010 Purpose of Design Review

Design review ensures that development conserves and enhances the recognized special design values of a site or area. Design review is used to ensure the conservation, enhancement, and continued vitality of the identified scenic, architectural, and cultural values of each design district or area. Design review ensures that certain types of infill development will be compatible with the neighborhood and enhance the area. Design review is also used in certain cases to review public and private projects to ensure that they are of a high design quality.

Section 33.825.055 Design Review Approval Criteria

A design review application will be approved if the review body finds the applicant to have shown that the proposal complies with the design guidelines for the area.

Findings: The site is designated with design overlay zoning (d), therefore the proposal requires Design Review approval. Because of the site's location, the applicable design guidelines are the Community Design Guidelines.

Community Design Guidelines

The Community Design Guidelines consist of a set of guidelines for design and historic design cases in community planning areas outside of the Central City. These guidelines address the unique and special characteristics of the community plan area and the historic and conservation districts. The Community Design Guidelines focus on three general categories: **(P) Portland Personality**, which establishes Portland's urban design framework; **(E) Pedestrian Emphasis**, which states that Portland is a city for people as well as cars and other movement systems; and **(D) Project Design**, which assures that each development is sensitive to both Portland's urban design framework and the users of the city.

Staff has considered all guidelines and has addressed only those guidelines considered applicable to this project.

P1. Plan Area Character. Enhance the sense of place and identity by incorporating site and building design features that respond to the area's desired characteristics and traditions.

D7. Blending into the Neighborhood. Reduce the impact of new development on established neighborhoods by incorporating elements of nearby, quality buildings such as building details, massing, proportions, and materials.

Findings for P1 & D7: The St. Johns Main Street and its immediate vicinity is comprised of an eclectic mix of utilitarian structures, such as former mechanic shops and gas stations now reused as retail spaces; classic streetcar era storefronts with commercial at the ground floor and residential above, and a few formal examples of classic civic architecture, represented in brick with Palladian windows in the original James Johns Elementary school, the library, and the Baha'i Center. The area has seen significant new residential development, but commercial development has been predominantly adaptive reuse of existing structures, with little new commercial construction near the site.

Significant analysis of the social and historic context of St. Johns, its civic spaces and its existing built context helped Staff determine that while a more classic tripartite design reminiscent of the streetcar era storefronts or the industrial and manufacturing buildings might be appropriate elsewhere in the North Lombard, that this site is unique in its plaza-frontage location, at the primary intersection and serving as a gateway to the neighborhood.

This site is at the geographic heart of the St. Johns neighborhood. Located at the intersection where all incoming traffic from the St. Johns bridge must turn left or right onto North Lombard, the site is set behind a deep public plaza and backed by an alley. It serves as a backdrop for the community farmers markets, music festivals, art fairs and parade celebrations that are hosted in the public plaza. Its iconic location also gives it a special responsibility to be a unique and special building. This is not the appropriate location for a staid backdrop building.

Like many other buildings lining N. Lombard, this proposal has large storefront windows at the ground floor, fostering views and connection with the activities within. Deep canopies over each glass bay provide pedestrian protection and facilitate spill-out activities such as café seating from the ground-floor tenant spaces. The canopies, with integrated lighting and wood soffits, serve to define the street level, and differentiate it from the upper levels.

Neither the applicant nor the Commission contend that this proposal is architecturally reminiscent of the existing context. However, its location – surrounded by a deep, well-landscaped and significant community space – indicate that this is a neighborhood location appropriate for an iconic, jewel-box of a building, that stands out from more utilitarian buildings and enhances the identity of a special place with notable and unique architecture. *Therefore, these guidelines are met.*

- E1. The Pedestrian Network.** Create an efficient, pleasant, and safe network of sidewalks and paths for pedestrians that link destination points and nearby residential areas while visually and physically buffering pedestrians from vehicle areas.
- E2. Stopping Places.** New large-scale projects should provide comfortable places along pedestrian circulation routes where people may stop, visit, meet, and rest.
- E3. The Sidewalk Level of Buildings.** Create a sense of enclosure and visual interest to buildings along sidewalks and pedestrian areas by incorporating small scale building design features, creating effective gathering places, and differentiating street level facades.
- E4. Corners that Build Active Intersections.** Create intersections that are active, unified, and have a clear identity through careful scaling detail and location of buildings, outdoor areas, and entrances.
- E5. Light, Wind, and Rain.** Enhance the comfort of pedestrians by locating and designing buildings and outdoor areas to control the adverse effects of sun, shadow, glare, reflection, wind, and rain.

Findings for E1, E2, E3, E4 and E5: The clean lines and verticality of the façade serve to accentuate the luminaire quality of the glass curtain wall oriel projections and the highly-transparent, permeable ground floor. The proposal's clear identity and highly visible location will demarcate the corner and serve to activate and illuminate the intersection and the public plaza.

At the street level, generous and well-detailed canopies frame tall storefront windows and folding glass doors that can create an entirely open corner facing into the plaza. These

operable glass panels link the interior commercial space with the activity in the public realm, and facilitate seating, stopping and resting spaces that take advantage of the plaza location while being buffered from street traffic, sun, wind and rain. Pedestrian-scale doors on each frontage incorporate small-scale building design features, and facilitate the demise of the ground floor to serve various tenant space needs without compromising the intended permeability or visibility of the street level. *Therefore, these guidelines are met.*

D1. Outdoor Areas. When sites are not fully built on, place buildings to create sizable, usable outdoor areas. Design these areas to be accessible, pleasant, and safe. Connect outdoor areas to the circulation system used by pedestrians;

D3. Landscape Features. Enhance site and building design through appropriate placement, scale, and variety of landscape features.

Findings for D1 and D3: While the proposal does not include any renovation to the public plaza, owned by the Portland Bureau of Transportation, its street-level is highly responsive to this plaza. The proposal was significantly altered in response to community comments that demonstrated a strong desire for direct access to interior commercial spaces from the plaza. In consideration of the seasonal nature of plaza usage, the applicant has located primary entrances closer to the street frontages and well-trafficked sidewalks, and has designed the corner to be operable in good weather to connect outdoor seating areas to the plaza and pedestrian realm. *Therefore, these guidelines are met.*

D2. Main Entrances. Make the main entrances to houses and buildings prominent, interesting, pedestrian accessible, and transit-oriented.

Findings for D2: The primary entrance into the large, corner retail space is located across from N. Burlington and very close to the #4, 16, 44 and 75 bus stops. North Lombard makes a slight turn at this area, resulting in the site having a slightly canted relationship with the street frontage. This location is significantly closer, and therefore more visible, to the street than the corner location which is set deep into the plaza and partially screened by low walls and mature landscaping. The year-round visibility, proximity to transit and the most common path of pedestrian travel along Lombard and a more contextually typical relationship with neighboring retail entrances makes this an appropriate location for the main entrance. *Therefore, this guideline is met.*

D4. Parking Areas and Garages. Integrate parking in a manner that is attractive and complementary to the site and its surroundings. Locate parking in a manner that minimizes negative impacts on the community and its pedestrians. Design parking garage exteriors to visually respect and integrate with adjacent buildings and environment.

D5. Crime Prevention. Use site design and building orientation to reduce the likelihood of crime through the design and placement of windows, entries, active ground level uses, and outdoor areas.

Findings for D4 and D5: The proposal includes no vehicle parking or loading spaces. However, it has treated bike parking in a manner that is attractive and complementary to the site and its surroundings. Rather than treating the alley as a back-of-house condition, the applicant has continued the pattern of alternating opaque and glazed bays around to the alley frontage, with large storefront window bays illuminating the alley and fostering views into the residential lobby and bike storage from North Philadelphia.

Additionally, the location of a bike entrance off the alley, rather than providing a singular residential lobby door, ensures that residents will regularly be entering and existing into the alley. This proposal will fundamentally transform that right-of-way from an under-lit and rarely used back of house, to a well-lit and highly active area. This treatment sets a context for future buildings to further enhance and activate the length of the alley. *Therefore, these guidelines are met.*

D8. Interest, Quality, and Composition. All parts of a building should be interesting to view, of long lasting quality, and designed to form a cohesive composition.

Findings for D8: The proposal is for a simple, well-detailed box, with alternating vertical bands of terra-cotta colored, high density cementitious rain screen panels, and vertical projecting glass boxes. The regular, cohesive design concept will be enriched by the variable nature of the activity within, and surrounding the building. The building serves as the backdrop for public activity at the plaza level, and for the residential and commercial activity within.

The projecting glass oriels utilize a curtain wall system that facilitates the desired glass jewel box effect without bulky vertical structural components complicating the façade. Per Condition of Approval C, all metal panel will be of a stiff enough gauge to prevent denting and/or pillowing due to extreme temperature fluctuations.

The proposal has utilized its alley frontage to create a three-sided building. While oriel projections were not allowed, per Portland Bureau of Transportation, the alley frontage shares the same vertical patterning of opaque rain screen and glass bays as the two street facades. The lone end wall, which will rise three stories above the adjacent one-story concrete building, utilizes varied panel widths to add detail and visual interest without creating a visual statement that might distract from the other buildings on the street.

At the street level, the thoughtful detailing will enhance the pedestrian experience. Canopies are four feet deep, in addition to 3' recesses for entry doors. Tongue and groove wood cladding, sealed in a dark charcoal color, provides tactile detailing at the ground level and is easily accessed for repair and maintenance. Wall-mounted lighting, sliding glass panel doors and tall storefront windows further enrich the pedestrian realm and contribute to the active, pedestrian-oriented Main Street environment. Per Condition of Approval D, exhaust vents will be located above canopies, to protect pedestrians from the negative impacts of air exhaust. *With Conditions C and D this guideline is met.*

33.825.040 Modifications That Will Better Meet Design Review Requirements:

The review body may consider modification of site-related development standards, including the sign standards of Chapters 32.32 and 32.34 of the Sign Code, as part of the design review process. These modifications are done as part of design review and are not required to go through the adjustment process. Adjustments to use-related development standards (such as floor area ratios, intensity of use, size of the use, number of units, or concentration of uses) are required to go through the adjustment process. Modifications that are denied through design review may be requested as an adjustment through the adjustment process. The review body will approve requested modifications if it finds that the applicant has shown that the following approval criteria are met:

- A. **Better meets design guidelines.** The resulting development will better meet the applicable design guidelines; and
- B. **Purpose of the standard.** On balance, the proposal will be consistent with the purpose of the standard for which a modification is requested.

Modification #1: Standards for all bicycle parking, 33.266.220.C.3.b - To reduce the width of wall-mounted hanging bicycle rack from 24 inches to 18 inches.

Purpose of Standard 33.266.220 – Bicycle Parking

These standards ensure that required bicycle parking is designed so that bicycles may be securely locked without undue inconvenience and will be reasonably safeguarded from intentional or accidental damage.

Findings for Modification 1: The proposal includes 37 long-term bicycle parking spaces for residents (33 are required) and 2 long-term bicycle parking spaces for retail employees (2 are required). All long-term bicycle parking spaces are accommodated in the

residential lobby area, with direct access from the alley. No required bicycle parking spaces are located within the residential units.

The Commission celebrates the thoughtful design of the long-term bicycle storage which supports the accommodation of all required bike parking outside of residential units while activating an otherwise underutilized alley with glazing, a bicycle entrance and activity. The wide, direct-access door from the alley to the bike parking area facilitates convenient and safe access, mitigating the accidental damage that may arise from bicyclists trying to navigate narrow, furnished pedestrian entrances. It also reduces the damage and inconvenience that often results when residents must bring bikes in the elevators to store them in residential units. Last, the frequent use of this attractive, glazed alley entrance by bicyclists serves to activate and enhance the alley right-of-way.

Accommodating 39 bicycle parking spaces in horizontal racks would consume considerable floor area at the ground floor and might reduce the available active ground-floor retail space. Relying on a vertical/wall hanging bike rack is a more efficient use of space, and is identical to the parking system recently approved in numerous Design Reviews throughout the City. The proposed functional and space efficient system better meets the design guidelines E1 – Pedestrian Network; E3: The Sidewalk Level of Buildings, and D4: Parking Areas and Garages because it eases floor plan demands and results in additional opportunities for needed residential units, while enhancing a public alley right-of-way with glazing, activity and lighting.

The proposed bike rack system is engineered to stagger bikes vertically to allow the handle bars to overlap. This allows the proposed racks, within an 18” space, to provide the same level of service that would be provided by a standard 24” on center spacing. A 5’ minimum aisle is still provided behind each bicycle rack. The rack system will be located within a secure residential lobby area, separate from the pedestrian entrance and seating area. For these reasons, the bicycle parking system is safe and secure, and designed to avoid any intentional or accidental damage to bicycles; as such, the proposal is consistent with the purpose statement of the bicycle parking standards. *Because the proposal better meets design guidelines E1: Pedestrian Network; E3: The Sidewalk Level of Buildings, and D4: Parking Areas and Garages, this Modification warrants approval.*

DEVELOPMENT STANDARDS

Unless specifically required in the approval criteria listed above, this proposal does not have to meet the development standards in order to be approved during this review process. The plans submitted for a building or zoning permit must demonstrate that all development standards of Title 33 can be met, or have received an Adjustment or Modification via a land use review prior to the approval of a building or zoning permit.

REVIEW BODY CONCLUSIONS

The design review process exists to promote the conservation, enhancement, and continued vitality of areas of the City with special scenic, architectural, or cultural value. The proposal meets the applicable design guidelines and modification criteria and therefore warrants approval.

The proposal sufficiently addresses its adjacent commercial context through the incorporation of extensive ground floor glazing, numerous retail entrances, deep canopies, ground-level material detailing and pedestrian-scale lighting. It addresses its unique and special civic plaza location through a unique fully-operable corner glazing element that allows the entire ground floor plaza corner to be opened, revealing a 38’ wide entrance. It addresses its two street frontages by surpassing the maximum ground floor glazing requirement on North Lombard Street, and achieving more than twice the required ground floor glazing required on North Philadelphia. Further, it brings glazing around to the ground level of the alley, enhancing the view from the St. Johns bridge and activating an otherwise underutilized public right-of-way.

The primary concerns of the appellant and public testifiers were the scale of glazing on upper floors, the cementitious cladding material and the lack of differentiation between ground floor

entrances. The Design Commission noted the very specific description in P1: Plan Area Character that directs new development on commercial streets in St. Johns to focus on activation of the public realm through outdoor seating, ground floor storefronts and integration with the public realm. The Commission determined that this proposal had met all the desired Plan Area characteristics as well as all relevant Community Design Guidelines, and therefore warranted approval.

REVIEW BODY DECISION

Approval of a four-story, 30-unit building with ground floor commercial space, per the approved site plans, Exhibits C-1 through C-43, signed and dated October 16, 2017, subject to the following conditions:

- A. As part of the building permit application submittal, the following development-related conditions (B through E) must be noted on each of the 4 required site plans or included as a sheet in the numbered set of plans. The sheet on which this information appears must be labeled "ZONING COMPLIANCE PAGE - Case File LU 17-113306 DZM." All requirements must be graphically represented on the site plan, landscape, or other required plan and must be labeled "REQUIRED."
- B. At the time of building permit submittal, a signed Certificate of Compliance form (<https://www.portlandoregon.gov/bds/article/623658>) must be submitted to ensure the permit plans comply with the Design/Historic Resource Review decision and approved exhibits.
- C. Metal panels to be a minimum of 22 gauge for spans less than 12", and a minimum of 20 gauge for spans of 12" or more. Backed, composite metal panel is an approvable substitute and may utilize any gauge of metal.
- D. All exhaust vents in entrance bays must vent above canopies.
- E. No field changes allowed.

Staff Planner: Hannah Bryant

First Hearing Date: December 14, 2017

Findings and conclusions by the Design Commission on: December 14, 2017

By:  Chair, Design Commission
Julie Livingston

Date Final Decision Effective/Mailed: December 29, 2017

120th day date: January 1, 2018

About this Decision. This land use decision is **not a permit** for development. Permits may be required prior to any work. Contact the Development Services Center at 503-823-7310 for information about permits.

Procedural Information. The application for this land use review was submitted on January 30, 2017, and was determined to be complete on July 25, 2017.

Zoning Code Section 33.700.080 states that Land Use Review applications are reviewed under the regulations in effect at the time the application was submitted, provided that the application is complete at the time of submittal, or complete within 180 days. Therefore, this application was reviewed against the Zoning Code in effect on January 30, 2017.

ORS 227.178 states the City must issue a final decision on Land Use Review applications within 120-days of the application being deemed complete. The 120-day review period may be waived or extended at the request of the applicant. In this case the applicant requested that the 120-day review period be extended for 40 days (Exhibits A.8 and A.11). Unless further extended by the applicant, **the 120 days will expire on: January 1, 2018.**

Appeal of this Decision. This decision is final and becomes effective the day the notice of decision is mailed (noted above). This decision may not be appealed to City Council; however, it may be challenged by filing a "Notice of Intent to Appeal" with the State Land Use Board of

Appeals (LUBA) within 21 days of the date the decision is mailed, pursuant to ORS 197.0 and 197.830. A fee is required, and the issue being appealed must have been raised by the close of the record and with sufficient specificity to afford the review body an opportunity to respond to the issue. For further information, contact LUBA at the Public Utility Commission Building, 550 Capitol Street NE, Salem, OR 97310. [Telephone: (503)373-1265]

Recording the Final Decision.

If this Land Use Review is approved the final decision will be recorded with the Multnomah County Recorder.

- *Unless appealed*, the final decision will be recorded after **December 29, 2017** by the Bureau of Development Services.

The applicant, builder, or a representative does not need to record the final decision with the Multnomah County Recorder.

For further information on your recording documents please call the Bureau of Development Services Land Use Services Division at 503-823-0625.

Expiration of this approval. An approval expires three years from the date the final decision is rendered unless a building permit has been issued, or the approved activity has begun.

Where a site has received approval for multiple developments, and a building permit is not issued for all of the approved development within three years of the date of the final decision, a new land use review will be required before a permit will be issued for the remaining development, subject to the Zoning Code in effect at that time.

Applying for permits. A building permit, occupancy permit, or development permit may be required before carrying out an approved project. At the time they apply for a permit, permittees must demonstrate compliance with:

- All conditions imposed herein;
- All applicable development standards, unless specifically exempted as part of this land use review;
- All requirements of the building code; and
- All provisions of the Municipal Code for the City of Portland, and all other applicable ordinances, provisions and regulations of the City.

EXHIBITS

NOT ATTACHED UNLESS INDICATED

A. Applicant's Submittals

1. Original Submittal, dated January 27, 2017
2. Envelope Compliance Certificate, dated May 3, 2017
3. Response to Incomplete Memo, dated July 24, 2017
4. Submittal 02, dated July 24, 2017
5. Submittal 03, dated September 8, 2017
6. Bulleted Response to SJNA Letter
7. Response to Neighborhood Concerns Memo, dated September 14, 2017
8. 30-day Extension, dated September 19, 2017
9. Submittal 04, dated October 5, 2017
10. Final Submittal Appendices, dated October 11, 2017
11. 10-day Extension, dated October 16, 2017

B. Zoning Map (attached)

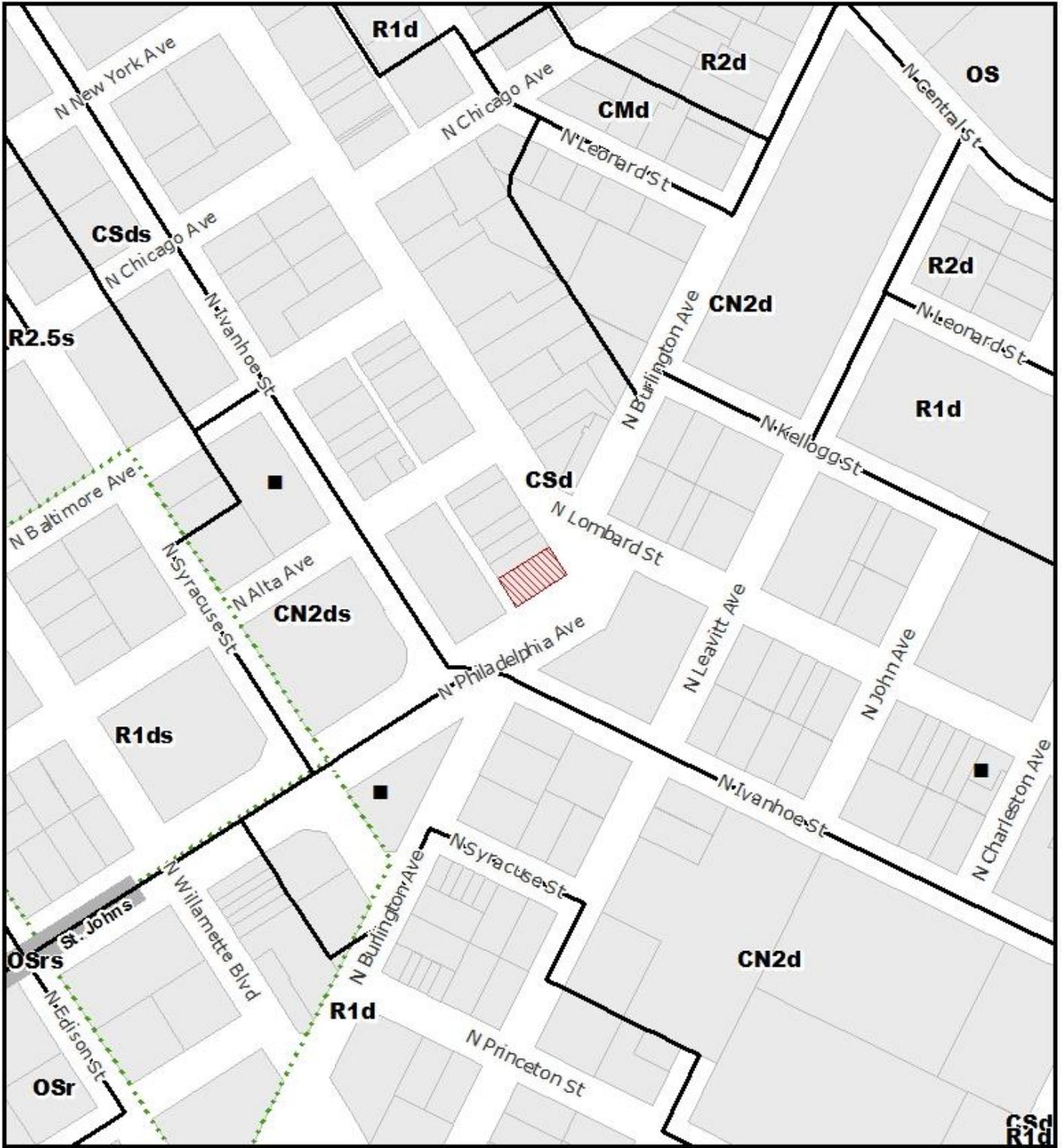
C. Plans/Drawings:

1. Index (not stamped)
2. Site Plan (attached)
3. Floor Plan – Ground Story
4. Floor Plan – Second Story Plan
5. Floor Plan – Third Story Plan
6. Floor Plan – Fourth Story Plan

7. Roof Plan
 8. North Elevation – Color
 9. North Elevation – Black and White
 10. East Elevation – Color
 11. East Elevation – Black and White
 12. South Elevation – Color
 13. South Elevation – Black and White
 14. West Elevation – Color
 15. West Elevation – Black and White
 16. East – West Section
 17. North – South Section
 18. Shear Wall Details
 19. Bay Window & Retail Entry Details
 20. Residential Entry Details
 21. Utility | Alley Details
 22. Canopy, Signage, Fastener Details
 23. Materials
 24. Bike Parking
 25. Exterior Lighting + Ventilation Plan
 26. Civil Existing Conditions Plan
 27. Civil Utility Plan
 28. Mechanical Cut Sheets
 29. Cembrit Patina Cladding Cut Sheet
 30. Curtain Wall Cut Sheet
 31. Curtain Wall Vent Cut Sheet
 32. Curtain Wall Vent Detail
 33. Flush Face Concealed Metal Panel Cut Sheet
 34. Aluminum Sliding Doors Cut Sheet
 35. Aluminum Storefront Cut Sheet
 36. Aluminum Entrances Cut Sheet
 37. ACM Soffit Panel Cut Sheet
 38. Wood Cladding Stain Cut Sheet
 39. Overhead Door Cut Sheet
 40. Recessed Can Light Cut Sheet
 41. Linear Wall Sconce Cut Sheet
 42. Linear Wall Sconce Cut Sheet
 43. LED Sign Light Cut Sheet
- D. Notification information:
1. Mailing list
 2. Mailed notice
- E. Agency Responses:
1. Bureau of Environmental Services
 2. Bureau of Transportation Engineering and Development Review
 3. Water Bureau
 4. Fire Bureau
 5. Site Development Review Section of BDS
- F. Correspondence:
1. Joshua Guerra, July 14, 2017, concerns about response to contextual architecture.
 2. Christa Holden, July 14, 2017, concerns about relationship to plaza.
 3. Holly Hagerman, September 23, 2017, does not feel building fits with main street.
 4. St. Johns Neighborhood Association, September 11, 2017, numerous concerns including architectural style, materials, response to plaza, pedestrian realm detailing.
- G. Other:
1. Original LU Application
 2. Site Photos
 3. Early Assistance Notes, dated March 28, 2016
 4. Incomplete Letter, dated February 24, 2017
 5. St. Johns Town Center, Appendix K, Community Design Guidelines
 6. St. Johns Plan District, Maximum Height, Map 583-2

- H. Received before the December 14, 2017 Appeal Hearing
1. Appeal Submittal – Rachel Hill, St. Johns Neighborhood Association
 2. Appealed Administrative Type II Notice of Decision/Notice of Appeal
 3. Mailing List
 4. James Muggenberg, November 1, 2017 – letter of support for proposal
 5. Stacey Van de Graeff, December 12, 2017 – letter of support for proposal
- Received at the December 14, 2017 Appeal Hearing
6. Staff PowerPoint Presentation to Hearing Body
 7. Appellant PowerPoint Presentation to Hearing Body
 8. Applicant PowerPoint Presentation to Hearing Body
 9. Testimony Sign-up Sheet

The Bureau of Development Services is committed to providing equal access to information and hearings. Please notify us no less than five business days prior to the event if you need special accommodations. Call 503-823-7300 (TTY 503-823-6868).

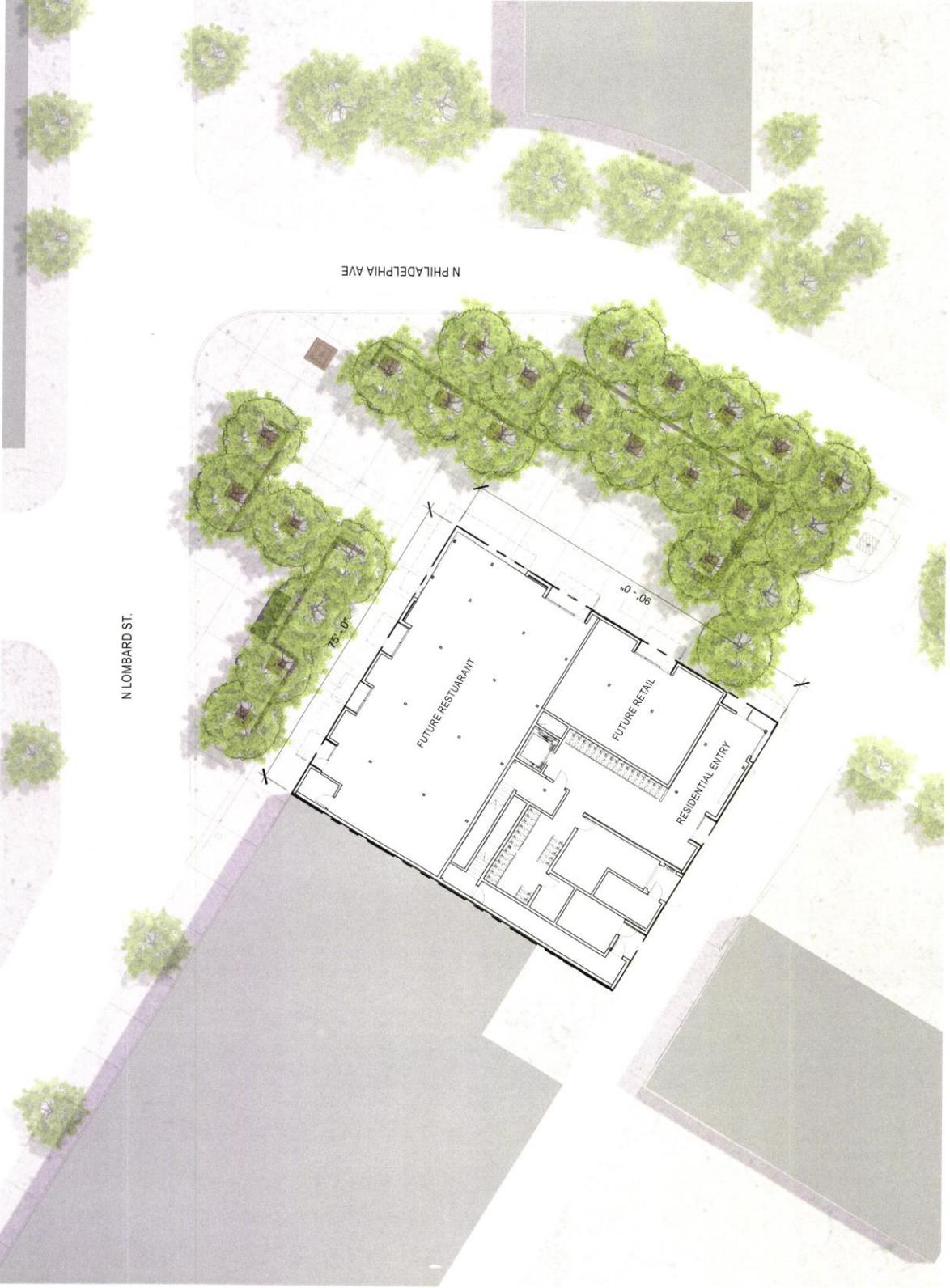


ZONING



-  Site
-  Historic Landmark
-  Bridge
-  Recreational Trails

File No.	LU 17-113306 DZM
1/4 Section	2121
Scale	1 inch = 200 feet
State ID	1N1W12BA 6600
Exhibit	B Aug 09, 2017



Approved
City of Portland - Bureau of Development Services
Planner *[Signature]*
Date 10-15-17
* This approval applies only to the reviews requested and is subject to all
conditions of approval. Additional review requirements may apply.

SITE PLAN - GROUND STORY
3/32" = 1'-0"



17-113306 C.2