



STAFF REPORT AND RECOMMENDATION TO THE DESIGN COMMISSION - DENIAL

CASE FILE: LU 17-111569 DZM (PC # 16-284738)
1327 NW 19th
REVIEW BY: Design Commission
WHEN: February 15, 2018, 1:30pm
WHERE: 1900 SW Fourth Ave., Room 2500B
Portland, OR 97201

Bureau of Development Services Staff: Grace Jeffreys 503-823-7840 /
Grace.Jeffreys@portlandoregon.gov

Please note this report does not recommend approval. Aspects of the proposal that do not meet the approval criteria are boxed and in underlined text, and a Summary of the Issues and Approval Criteria not met are in the "Conclusions" section.

GENERAL INFORMATION

Applicant: Amy Thornton, YB-A Architects PC
123 NW 2nd Ave, Suite 204, Portland, OR 97209
(971) 888-5107, amy@yb-a.com

Owner: DD Pettygrove, LLC
901 NE Glisan St. Suite 100, Portland, OR 97232
Ryan Schera, (503) 297-8791, ryan.schera@deacon.com

Site Address: **1323-1327 NW 19TH AVE**

Legal Description: BLOCK 265 LOT 1&2, COUCHS ADD; BLOCK 265, CANCEL INTO R141114 / COUCHS ADD, BLOCK 265, LOT 2

Tax Account No.: R180224010, R180224030, R180224030

State ID No.: 1N1E33AB 09900, 1N1E33AB 09800, 1N1E33AB 09800

Quarter Section: 2928

Neighborhood: Northwest District, contact John Bradley at 503-313-7574.

Business District: Nob Hill, contact Nob Hill at nobhillportland@gmail.com.

District Coalition: Neighbors West/Northwest, contact Mark Sieber at 503-823-4212.

Plan District: Northwest

Zoning: **EXd**, Central Employment (EX) with Design (d) Overlay

Case Type: **DZM**, Design Review with Modifications

Procedure: **Type III**, with a public hearing before the Design Commission. The decision of the Design Commission can be appealed to City Council.

Proposal:

Applicant seeks **Design Review** approval for a new 7-story mixed-use building with 107 market-rate residential units above ground level retail (1,150 SF), services, bike room and vehicle parking, located in the Northwest Plan District. Thirty-four (34) mechanical car parking spaces, one ADA parking space, two (2) Type B loading spaces, and 120 long-term bike parking spaces will be provided. Exterior materials include ribbed and flat metal paneling, black vinyl windows, glass guardrails, steel plate canopies and planters, formed concrete piers, black aluminum storefronts and wood doors and seating accents.

Five (5) **Modifications** to Development Standards of Title 33 are requested:

1. **Height Bonus for Residential Development** (33.562.230.D). Maximum building height allowed is 75'. The project proposes to increase maximum height by 3'-8", to 78'-8"
2. **Standards for all Bicycle Parking** (33.266.220.C.3.b). The project proposes to use double stack bike racks for 118 of the long-term bike parking spaces, spaced at 1'-6" rather than the 2' spacing required.
3. **Loading Standards, Forward Motion** (33.266.310.F.1). The project proposes that loading access for the two Standard B spaces be forward ingress/egress in one direction, rather than both directions.
4. **Parking Area Layouts** (33.266.130.F.1). The project proposes to locate on-site loading in the driveway aisle, temporarily blocking 24 of the mechanical entry parking stalls as well as the elevator control room door.
5. **Size of Auto Parking Spaces** (33.266.130.F.2) The project proposes to decrease the width of the 34 mechanical parking spaces to 8'-0" wide, rather than the 8'-6" spacing required.

One (1) **Modification** to Development Standards of Title 33 has been further identified by staff:

6. **Ground Floor Windows** (33.140.230). Staff identified that the proposal provides 40% rather than the required 50% minimum length required of qualifying windows.

Project Summary:

- Site Area 10,000 square feet
- Total GSF proposed 58,053 square feet
- Max FAR allowed 6:1 (4:1 with 2:1 bonus for small site per 33.562.230.C)
- Actual FAR 5.81:1
- Max Height allowed 75'-0" (45' with up to 75' with housing per 33.562.230.D)
- Actual Height 78'-8" (Modification request of 3'-8" of additional height)

Bike parking.

- Long-term bike parking will be met within the ground level bike room (118 spaces) and the ground floor retail space (2 spaces). A Modification is requested to the spacing of the racks, see Modification #2.
- Short-term bike parking is not provided and the project will be paying into the bike fund.

Amenities.

- Shared amenity space is provided for residents in the 2nd floor amenity room with an adjoining exterior 2nd floor terrace.
- Glass guardrails at Levels 2, 4, and 6 will act as Juliette balconies for some of the units.

Because the proposal is for new construction in a design overlay zone, Design Review is required prior to the issuance of building permits.

Relevant Approval Criteria:

In order to be approved, this proposal must comply with the approval criteria of Title 33, Portland Zoning Code. The relevant approval criteria are:

- 33.420, Design Overlay
- 33.562, Northwest Plan District
- 33.825, Design Review
- 33.825.040, Modifications That Will Better Meet Design Review Requirements
- Community Design Guidelines

ANALYSIS

Site and Vicinity: The site is located at the corner of NW 19th Avenue and NW Pettygrove Street in the Northwest Plan District. The site is comprised of two lots with a total of 10,000 SF. The site is currently developed with three existing structures that will be demolished. Two were listed on the City of Portland’s Historic Resource Inventory (HRI), and have since been removed:

- 1323 NW 19th Ave was listed as a Rank III resource on the HRI, and was constructed in 1884 in the Queen Anne Vernacular style.
- 1333 NW 19th was listed as a as a Rank III resource on the HRI, and was constructed in 1884 in the Italianate, Western Falsefront Vernacular style.

The site is bordered by two developed parcels: to the west is a one-story industrial building set back from the street with surface parking, and to the south is a recently completed six-story apartment building, the “Muse”.

Portland’s Transportation System Plan classifies NW 19th Avenue as a Transit Access Street and City Bikeway, and NW Pettygrove as a as a Local Service Street for all modes. The site is within the Northwest Pedestrian District and the Portland Streetcar alignment is to the south of the site at NW Northrup Street.

NW 19th Ave is a boundary street at the transition from the commercially mixed Northwest Triangle to the east and the residentially based Alphabet District to the west. NW Pettygrove St is predominately residential in character with two- to three-story walk up brick town homes, and larger multi-resident dwellings.

The site is within the “Transition Area” Urban Character Area “C” outlined in the Northwest District Plan. While pockets of the Transition Area include pre-World War II buildings, such as a mix of small apartment buildings and detached houses, much of the area is characterized by large-scale institutional and industrial buildings built since World War II.

Zoning: The Central Employment (EX) zone allows mixed uses and is intended for areas in the center of the City that have predominantly industrial-type development. The intent of the zone is to allow industrial and commercial uses which need a central location. Residential uses are allowed, but are not intended to predominate or set development standards for other uses in the area.

The Design Overlay Zone [d] promotes the conservation, enhancement, and continued vitality of areas of the City with special scenic, architectural, or cultural value. This is achieved through the creation of design districts and applying the Design Overlay Zone as part of community planning projects, development of design guidelines for each district, and by requiring design review. In addition, design review ensures that certain types of infill development will be compatible with the neighborhood and enhance the area.

The Northwest Plan District implements the Northwest District Plan, providing for an urban level of mixed-use development including commercial, office, housing, and employment. Objectives of the plan district include strengthening the area's role as a commercial and residential center. The regulations of this chapter: promote housing and mixed-use development; address the area's parking scarcity while discouraging auto-oriented developments; enhance the pedestrian experience; encourage a mixed-use environment, with transit supportive levels of development and a concentration of commercial uses, along main streets and the streetcar alignment; and minimize conflicts between the mixed-uses of the plan district and the industrial uses of the adjacent Guild's Lake Industrial Sanctuary.

Land Use History: City records do not indicate any relevant prior land use reviews.

Agency Review: A "Request for Response" was mailed **December 22, 2017**. The following Bureaus have responded with no issue or concerns:

- **Bureau of Environmental Services** (Exhibit E-1)
- **Bureau of Transportation Engineering** (Exhibit E-2)
- **Fire Bureau** (Exhibit E-3)
- **Bureau of Parks-Forestry Division** (Exhibit E-4)
- **Life Safety Review Section of BDS** (Exhibit E-5)

Neighborhood Review: A "Notice of a Public Hearing on a Proposal in Your Neighborhood" was mailed on **January 24, 2018**. One written response has been received from either the Neighborhood Association or notified property owners in response to the proposal.

- Katie Pitello, January 26, 2018, concern with amount of parking (Exhibit F-1).

Staff Response: There are no minimum car parking requirements for this development, however, the proposal does provide 35 car parking spaces.

ZONING CODE APPROVAL CRITERIA

(1) DESIGN REVIEW (33.825)

Section 33.825.010 The Purpose of Design Review

Design Review ensures:

- That development conserves and enhances the recognized special design values of a site or area;
- The conservation, enhancement, and continued vitality of the identified scenic, architectural, and cultural values of each design district;
- That certain types of infill development will be compatible with the neighborhood and enhance the area; and
- High design quality of public and private projects.

Section 33.825.055 Design Review Approval Criteria

A design review application will be approved if the review body finds the applicant to have shown that the proposal complies with the design guidelines for the area.

It is important to emphasize that design review goes beyond minimal design standards and is viewed as an opportunity for applicants to propose new and innovative designs. The design guidelines are not intended to be inflexible requirements. Their mission is to aid project designers in understanding the principal expectations of the city concerning urban design.

The review body conducting design review may waive individual guidelines for specific projects should they find that one or more fundamental design guidelines is not applicable to the circumstances of the particular project being reviewed.

The review body may also address aspects of a project design which are not covered in the guidelines where the review body finds that such action is necessary to better achieve the goals and objectives of design review in the Central City.

Findings: The site is designated with design overlay zoning (d). Therefore, the proposal requires Design Review approval. Because the site is within the Northwest Plan District, the applicable approval criteria are listed in the Community Design Guidelines.

Community Design Guidelines

The Community Design Guidelines consist of a set of guidelines for design and historic design cases in community planning areas outside of the Central City. These guidelines address the unique and special characteristics of the community plan area and the historic and conservation districts. The Community Design Guidelines focus on three general categories: **(P) Portland Personality**, which establishes Portland's urban design framework; **(E) Pedestrian Emphasis**, which states that Portland is a city for people as well as cars and other movement systems; and **(D) Project Design**, which assures that each development is sensitive to both Portland's urban design framework and the users of the city.

Staff has considered all guidelines and has addressed only those guidelines considered applicable to this project. Additionally, staff has organized the findings under three tenets, "Context", "Public Realm", and "Quality and Permanence".

CONTEXT

P1. Plan Area Character. Enhance the sense of place and identity by incorporating site and building design features that respond to the area's desired characteristics and traditions.

Findings: The site is located within the Northwest Plan District, and within the Urban Character Area C, the Eastern Edge. The site is also located in the Northwest Pedestrian District. NW 19th is a City Bikeway and is considered a Transit Access Street.

According to the Community Design Guidelines, Appendix J, this area is characterized by a diverse assortment of architectural types and is a diverse, mixed-use area with a fine-grain mixture of employment, residential, and community services. This area serves as a transition and connection between the residential core of the Northwest District and the more intensely developed Central City. This area includes examples of the residential structures from the late nineteenth-century middle and working-class neighborhoods as well as many early to mid-twentieth century light industrial buildings as well as a notable religious institution, St. Patrick's Church.

The desired characteristics and traditions of the Urban Character Area C expresses the intention to contribute to the architectural diversity of the Eastern edge and continue the pattern of partial block building massing. Additional characteristics are described: buildings lining street frontages rather than parking and the creation of an identifiable corridor along NW 19th Avenue.

According to the Community Design Guidelines, this guideline can be accomplished by:

A. Continuing the area's established pattern of partial block building massing. The proposed building massing is modulated to break the façade into various lengths both horizontally and

vertically. This is accomplished by shifting facade elements, using setback variations, and incorporating material changes to create a dynamic and interesting building massing.

C. Maintaining and re-establishing the area's historic street grid. This development will maintain the existing street grid, recognizing primary and secondary streets, by placing vehicle and pedestrian entrances appropriately as well as placing uses appropriate to each frontage. No changes to the existing grid are proposed.

D. Orienting the primary entrances, lobbies, and activity areas to the surrounding neighborhood instead of interior streets. While this goal is intended primarily for large multi-block developments, many of the sentiments are appropriate for smaller infill developments. This development places the primary entrances to retail and the apartment lobby along the faces of NW Pettygrove Street and NW 19th Avenue.

This guideline is therefore met.

D7. Blending into the Neighborhood. Reduce the impact of new development on established neighborhoods by incorporating elements of nearby, quality buildings such as building details, massing, proportions, and materials.

Findings: *According to the Community Design Guidelines, this guideline can be accomplished by:*

A. Incorporating elements and details found in nearby structures.

B. Divide large walls into distinct smaller planes that are more in keeping with the scale of surrounding development.

The evolving nature of this neighborhood has created a unique building environment that results in a blend of architectural styles and uses. This structure dwells within an architectural "Eastern Edge" as identified in Appendix J of the CDG. The site to the immediate south, the 'Muse', represents a forthcoming wave of high density urban housing with active ground floor retail. To its east, lies the raised portion of I-405 which represents the divide between the Pearl District and NW Portland. To the south and southwest of the site lie remnants of older warehousing and industrial uses with midblock massing's, as well as older residential homes. This creates a unique blend of building typology and usage that is architecturally diverse and dense.

The proposed building incorporates elements of the Eastern Edge district by utilizing midblock massing in the design of the façade along both NW 19th Avenue and NW Pettygrove Street. The small site inherently lends itself to avoiding long monotonous horizontal walls, and additional vertical planes and breaks in the building datum will serve to further divide the mass into smaller planes congruous with the historic fabric, preserving the overall pedestrian character of the Eastern Edge district. The design also serves to enhance the pattern of architectural variety that is characteristic of the Eastern Edge sub district by providing active ground floor uses.

This guideline is therefore met.

PUBLIC REALM

E1. The Pedestrian Network. Create an efficient, pleasant, and safe network of sidewalks and paths for pedestrians that link destination points and nearby residential areas while visually and physically buffering pedestrians from vehicle areas.

Findings: *According to the Community Design Guidelines, this guideline can be accomplished by:*

A. Providing safe, attractive, and convenient pedestrian connections and transitions from sidewalks to building entrances. On NW Pettygrove Street as well as NW 19th Avenue, the building entrances are set back from the property line, giving pedestrians space to transition from the sidewalk into the building. There will be a larger recess at the residential lobby entrance, which also features ornamental planters that contain vertical vine planting. An adjacent seating area gives pedestrians a place to pause outside of the pedestrian zone and entry while still maintaining direct, visually clear, and easy access.

D. Providing space for different activities that take place along sidewalks and walkways. The project will be required to reconstruct the sidewalk corridors along the frontages on NW Pettygrove Street and NW 19th Avenue as well as a new curb extension at NW Pettygrove Street and NW 19th Avenue. The 12-foot sidewalk corridor will provide a building frontage zone, a pedestrian zone, a furnishing zone, and a curb zone. These zones will provide a convenient and consistent access route for pedestrian travel along these streets. The new curb extension at NW Pettygrove Street and NW 19th Avenue will create additional space for pedestrians. The street frontages, as well as the new curb extension, will be constructed in compliance with ADA standards.

E. Using a variety of paving textures and patterns to separate motor vehicles, pedestrians, and bicycles. Access to the parking garage is located on NW Pettygrove Street, which is the less active of the two streets. The parking garage is also located away from the corner of NW Pettygrove Street and NW 19th Avenue, allowing this intersection to be more bicycle and pedestrian-oriented. A slightly recessed, perforated, coiling garage door, as well as the window into the garage, will allow for some visual cues as to when a vehicle will be exiting the garage, maintaining a safer pedestrian experience.

Long term bicycle parking is located at the south end of the parking garage, allowing for a bicyclist to enter the garage from the street and continue into the bike room without moving through a series of other doors. This bike room has entrances to the ground floor retail and lobby corridor, allowing for either residents or employees to leave their bike and conveniently walk to their respective destinations, without continuing back through the parking garage. The short-term bicycle parking will be met by paying into the biking fund.

The sidewalk on NW Pettygrove and NW 19th will utilize paving to differentiate the furnishing zone from the concrete pedestrian through zone and the building frontage zone. The pedestrian zone will be scored to clearly delineate the pedestrian areas from the vehicular entrance along NW Pettygrove Street, giving further indication of the potential for a vehicle to exit the garage.

This guideline is therefore met.

E2. Stopping Places. New large-scale projects should provide comfortable places along pedestrian circulation routes where people may stop, visit, meet, and rest.

Findings: *According to the Community Design Guidelines, this guideline can be accomplished by:*

C. Incorporating seating opportunities in the design of planters and walls.

E. Providing benches that are durable and impart a sense of permanence.

The proposed design provides a custom bench adjacent to the lobby entry off NW Pettygrove, which is adjacent to ornamental vine planters. This custom wood bench will wrap around the glazed corner of the building, and be fully protected from the weather with a continuous 5' deep canopy. The corner bench will extend from the edge of the retail to the residential lobby entry and serve as a natural and inviting transition between uses as well as from the public to

private realm of the lobby. Wrapping the corner of the building at the corner will also help to activate the ROW intersection and encourage pedestrian interaction and community engagement with the building.

This guideline is therefore met.

E3. The Sidewalk Level of Buildings. Create a sense of enclosure and visual interest to buildings along sidewalks and pedestrian areas by incorporating small scale building design features, creating effective gathering places, and differentiating street level facades.

Findings: *According to the Community Design Guidelines, this guideline can be accomplished by:*

A. Differentiating between the building façade at the sidewalk level and the floors above. The proposed building design features massing setbacks on both the north and east facades at the ground floor near active spaces, showing a clear distinction between the ground floor façade from the floors above. The degree to which the building step backs also varies to create an interest and spaces to stop, rest and enter the building. In addition, a continuous overhang above the ground level at the street facades help to create a horizontal datum separating it from the floors above. The ribbed metal panel on the upper building façade transitions to rib-formed concrete panels at the ground level, providing visual separation and a more durable ground floor material for pedestrian interaction.

C. Placing building walls, columns, and trees to create a sense of enclosure within the pedestrian path. A continuous canopy runs the entire length of the street-facing facades, creating a pedestrian path and sense of enclosure. Recessed building entries also add a sense of separation from the rest of the street activity. Additionally, set within the sidewalk furnishing zone of the north and east sides of the building are street trees of sufficient size (when mature) to create a sense of enclosure along the pedestrian path. In addition to the continuous overhang on NW Pettygrove and NW 19th, there are also recessed retail and lobby entrances which serves to reinforce the sense of pedestrian path enclosure.

D. Locating active indoor uses in areas with ground floor windows adjacent to sidewalks and public places. All the active uses of the building are located along the street faces of the building, adjacent to the sidewalk. On NW 19th Avenue, retail and lobby spaces cover most of the façade except for the pump room and stair access door located mid-block. On NW Pettygrove Street, the lobby and leasing office activate the façade closer to the corner, and the garage door and window into mechanical parking have been located mid-block. The large storefront windows at the active uses help reinforce the public nature of the ground floor. Less active spaces like the trash, electrical rooms, and bike storage are located away from the street face. Large storefront windows are also proposed in locations that will provide open views into service spaces: on NW 19th to the south of the retail spaces, windows are proposed at the pump room; and on NW Pettygrove Street to the west of the garage door, windows will provide some level of visibility into the mechanized parking. In both these locations; however, vertical vines are proposed which hopefully will help obscure these views.

E. Placing display windows along pedestrian paths. The retail spaces on NW 19th Avenue incorporate large, inviting storefront windows, as do the lobby space and offices on NW Pettygrove Street. These allow for the pedestrian experience to blend seamlessly into the experience inside the retail and lobby spaces.

This guideline is therefore met.

E4. Corners that Build Active Intersections. Create intersections that are active, unified, and have a clear identity through careful scaling detail and location of buildings, outdoor areas, and entrances.

Findings: *According to the Community Design Guidelines, this guideline can be accomplished by:*

A. Providing access to the interior of the building at the corner. The main lobby entrance is on NW Pettygrove Street near the corner, providing easy access for residents traveling along either NW Pettygrove Street or NW 19th Avenue. One of the retail entry doors on NW 19th Avenue is also within 40 feet of the intersection, providing another convenient entry for pedestrians coming from either direction.

B. Reinforcing the intersection by placing the highest or most interesting portion of the building near the corner. The building's ribbed metal panel façade is broken into interesting segments by break metal around the entirety of the building. This ribbed metal panel is angled on the street-facing facades, furthering the play of light and shadow even more. At the ground level, the continuous canopy and custom bench wraps the corner, creating a pedestrian zone at the intersection. The trellis vine planting and ribbed concrete panels further add to that pedestrian experience.

C. Locating parking to the side or rear of the site and bring the building up to the corner. Parking lots on corners weaken the structure of the intersection. The vehicle parking entrance has been placed along the building's NW Pettygrove Street facade on the ground floor, substantially set back from the corner. The parking extends away from the frontages and away from the street corner to allow the building to be built out to the street edge.

E. Extending curbs to shorten the distance across the street and create a larger sidewalk area. Curb extensions at the corner of NW Pettygrove Street and NW 19th Avenue will create a stronger definition of the corner at pedestrian level, while also providing a generous sidewalk area and creating a safer pedestrian crossing at the intersection.

This guideline is therefore met.

E5. Light, Wind, and Rain. Enhance the comfort of pedestrians by locating and designing buildings and outdoor areas to control the adverse effects of sun, shadow, glare, reflection, wind, and rain.

Findings: *According to the Community Design Guidelines, this guideline can be accomplished by:*

A. Providing weather protection for pedestrians at building entrances and over pedestrian paths such as arcades, awnings, canopies, porches, and overhangs. This building will provide pedestrians protection and comfort from the elements by providing an overhang and canopy that covers the entire ground floor frontage. In addition to the building overhang are a series of recessed entries that offer additional shelter when entering and exiting the building. The landscaping elements and street trees will also provide relief from sun, wind, and rain, as well as help to diffuse glare and reflections from the building's glazing.

This guideline is therefore met.

D2. Main Entrances. Make the main entrances to houses and buildings prominent, interesting, pedestrian accessible, and transit-oriented.

Findings: *According to the Community Design Guidelines, this guideline can be accomplished by:*

D. Using architectural elements, massing, and landscaping to accentuate the front entry. The proposed design places the main residential entrance along NW Pettygrove Street and articulates its presence through several architectural elements. The slight building setback and recess at the ground floor accentuates a difference from the rest of the building above, and the added glazing also serves to further accentuate the main entrance from its surroundings. The design also features vine planters along the main entrances on NW 19th Avenue and NW Pettygrove, adjacent to two custom benches that wrap the northeast corner, meant to promote stopping and provide pedestrian comfort. This vine planting stretches to the second level and provides further visual articulation of the main entrances.

This guideline is therefore met.

D3. Landscape Features. Enhance site and building design through appropriate placement, scale, and variety of landscape features.

Findings: *According to the Community Design Guidelines, this guideline can be accomplished by:*

C. Protecting and planting street trees. The proposed project will provide street trees along NW 19th Avenue to reinforce the definition of the pedestrian sidewalk enclosure. Along NW Pettygrove, however, no street trees are shown. Applicant is encouraged work with Urban Forestry to further explore opportunities to provide street trees on Pettygrove during the Building Permit process.

D. Using plant materials along sidewalks to define routes, buffer pedestrians from moving vehicles, create gateways, provide interest, color and texture. The proposed landscaping provides for a visually interesting experience for the pedestrian. The planting buffer in the furnishing zone along the NW Pettygrove Street and NW 19th Avenue creates a barrier from traffic and creates a clear pedestrian zone. This is accentuated by landscape amenities such as the planters along the frontage zone of NW 19th Avenue.

This guideline is therefore met.

D4. Parking Areas and Garages. Integrate parking in a manner that is attractive and complementary to the site and its surroundings. Locate parking in a manner that minimizes negative impacts on the community and its pedestrians. Design parking garage exteriors to visually respect and integrate with adjacent buildings and environment.

Findings: *According to the Community Design Guidelines, this guideline can be accomplished by:*

D. Providing a clear pedestrian path that connects parking areas with destination points. The parking garage entrance is located on NW Pettygrove Street, which is the less active street to minimize the impact on the pedestrian experience. The high-speed coiling door into the garage helps to avoid cars queuing over the sidewalk to enter the garage. Inside the garage, an entrance to the building interior is located near the center of the space to provide easy access to the lobby and mail area, as well as proximity to the elevator and stairs.

H. Screening indoor parking from pedestrians. The ground floor parking garage entrance has been minimized by its placement along NW Pettygrove Street, away from the intersection. The singular entrance and exit point to the garage concentrates vehicular access, allowing for a

safer and more predictable interaction between the vehicle and pedestrian traffic. The garage entrance is screened with a perforated coiling door that also allows for mechanical venting. The door is still approximately 75% opaque, providing visual screening but also allowing for some muted light to spill out at night for pedestrian safety.

In addition to the 20' wide coiling garage door, large storefront windows to the west of the garage door on NW Pettygrove Street open directly into the mechanized parking. While some vertical vines are proposed to partially obscure this view, staff is concerned that views into a parking structure are not an attractive or complementary resolution. And, as noted elsewhere in this report, this elevation does not meet the minimum standard for Ground Floor Windows required by the code. Further attention is required to this elevation to meet both the standards, as well as the Design Guidelines at the ground floor level of this elevation.

This guideline is therefore not yet met.

D5. Crime Prevention. Use site design and building orientation to reduce the likelihood of crime through the design and placement of windows, entries, active ground level uses, and outdoor areas.

Findings: *According to the Community Design Guidelines, this guideline can be accomplished by:*

A. Providing a lighting system that includes pedestrian-scale lights along walkways, energy-efficient porch and backyard lights that can be left on over time, and motion sensor lights that do not shine in rooms. Recessed soffit down-lighting will be placed along the ground floor street face at all exterior entries. These lights are intended to provide sufficient security light without introducing glare on adjacent properties. At the parking garage, the window and perforated garage door will allow light through at night to add to the safety around the building. Accent lighting including in-ground landscape lights at the vine planting and recessed linear lighting under the custom bench will encourage use of these spaces, create a safer atmosphere, and ultimately add to the overall ambient light quality.

B. Locating windows in active rooms and entrances to promote "eyes" on streets, plazas, and other shared out-door areas. This project has a ground floor presence on both streets and at the corner, with active use spaces located adjacent to the pedestrian path around the building. In that way, it provides 'eyes' on the street for crime prevention through design. NW Pettygrove Street benefits from full height storefront windows at the residential lobby and storefront windows into the leasing office. The ground floor façade along NW 19th Avenue also employs large storefront areas that connect the activity of the ground floor retail space to the activity of the street.

This guideline is therefore met.

QUALITY AND PERMENANCE

D8. Interest, Quality, and Composition. All parts of a building should be interesting to view, of long lasting quality, and designed to form a cohesive composition.

Findings: *According to the Community Design Guidelines, this guideline can be accomplished by:*

- A. Using cast stone, brick, terracotta and other long-lasting quality*
- B. Using a variety of textures and colors in exterior finish materials.*
- C. Incorporate details that add interest to buildings.*

- D. *Using architectural details that embellish the design of buildings.*
- E. *Using architectural details that embellish the design of buildings, such as ornamental columns, decorative brackets, and extensive use of trim to mark building edges.*
- F. *Using windows that embellish the character of buildings.*
- G. *Using materials and design features that promote quality and interest.*

The principal facades for the development incorporate a variety of high-quality, durable materials including:

At the ground level:

- Black aluminum storefront system with butt-glazed curtain wall at the lobby;
- Stained concrete panels with profiles reflecting box-ribbed panels above;
- Steel plate canopies and planter boxes;
- Steel cable trellis with vines; and,
- Wood accents at entry doors and benches.

Above the ground level:

- 20 gauge, concealed fastener box-rib and flat metal panels;
- Black commercial-grade vinyl windows; and,
- Dry-glazed glass railings.

This modern language of materials provides clean lines and interesting proportions. The silver ribbed box metal panel façade shifts in plane revealing dark metal panel behind, creating visual interest and breaking up the building's horizontal mass. These vertical ribbed box metal segments alternate between single and double story height, activating the façade with light and shadow, adding interest to the building massing. The angled box metal panels above transitions to durable concrete panel at the ground floor to create an ordered yet interesting pedestrian experience that will be resilient enough to withstand additional pedestrian interaction. Main entrance doors are accentuated and softened by wood.

The architectural textures created by the various cladding treatments will be juxtaposed with softer green vines along select locations at each major entry along the north and east faces. The vertical planting will assist in screening of spaces along NW Pettygrove and NW 19th, shading from solar gain, and provide seasonal visual interest and a softer texture in contrast to the harder architectural materials.

This guideline is therefore met.

(2) MODIFICATION REQUESTS (33.825)

33.825.040 Modifications That Will Better Meet Design Review Requirements:

The review body may consider modification of site-related development standards, including the sign standards of Chapters 32.32 and 32.34 of the Sign Code, as part of the design review process. These modifications are done as part of design review and are not required to go through the adjustment process. Adjustments to use-related development standards (such as floor area ratios, intensity of use, size of the use, number of units, or concentration of uses) are required to go through the adjustment process. Modifications that are denied through design review may be requested as an adjustment through the adjustment process. The review body will approve requested modifications if it finds that the applicant has shown that the following approval criteria are met:

- A. *Better meets design guidelines. The resulting development will better meet the applicable design guidelines; and*
- B. *Purpose of the standard. On balance, the proposal will be consistent with the purpose of the standard for which a modification is requested.*

The following five (5) Modifications are requested:

1. Height Bonus for Residential Development (33.562.230.D).

Standard. In bonus area A shown on Map 562-6, where at least 50 percent of the gross building area is in residential uses, the building may be up to 75 feet in height. Areas shared by residential and nonresidential uses are included in nonresidential floor area.

Proposal. The project requests to modify the allowable height of 75 feet (75') with an increase of 3 feet 8 inches (3' - 8") to a total of 78 feet 8 inches (78' - 8").

Purpose.

33.562.200 Development Standards for the Northwest Plan District. These development standards foster a transit-supportive, mixed-use urban character with a high-quality pedestrian environment, and an emphasis on good building design.

33.562.230.D. Height Bonus for Residential Development. Bonus options encourage certain uses and types of development that are desired within portions of the Northwest plan district and that implement the Northwest District Plan. The various bonus options encourage residential development, including housing affordable to a range of households; provide incentives for underground parking; and allow taller buildings to screen raised portions of the I-405 freeway.

Findings:

- A. *The resulting development will better meet the applicable design guidelines.* Four principles that support a proposed increased height include:
1. Improve the overall building massing proportions, and define a relationship to neighboring buildings including The Muse & other single-story warehouse buildings in the area.
 2. Increase the retail interior clear height by 3'-8", to 13' - 1" total.
 3. Allow for triple-stacking cars in the garage, increasing the parking count from 22 to 34.
 4. Create a tall podium to create a balanced visual proportion and weight of the base to the upper stories. Early design feedback from Staff stressed the importance of a heightened base that did not feel visually compressed. The design team chose to add 4 feet to the podium to achieve better overall massing.

By elevating the podium, the building achieves a better visual relationship to the adjacent building along 19th Avenue, the Muse, which has a double-height podium with a mezzanine added to the ground floor for residential units. In contrast, this proposal has avoided adding any residential to the ground floor, and instead provides commercial/retail spaces, which will help create a more active pedestrian environment.

The proportion of the podium gives the ground floor a grander scale that enhances the pedestrian experience with a sense of quality and permanence, emphasized by the materials, including stained concrete panels with formed vertical ribbing and glazing and generous storefront glazing. The interior retail and lobby spaces will maintain height to connect outside to inside, which will also help create a more active pedestrian environment.

The proposal also provides occupants with some on-site parking options. Parking is not required by zoning code; however, the project seeks to ease the burden of additional on-

street parking to the neighborhood. To maximize the number of parking spaces that can be provided, the project proposes to have mechanized parking allowing for 34 cars. The required height for the mechanized parking could not be achieved without the additional height at the podium.

For these reasons, the proposal better meets the following applicable approval criteria: *D7. Blending into the Neighborhood, E3. The Sidewalk Level of Buildings* and *D8. Interest, Quality, and Composition*.

- B. On balance, the proposal will be consistent with the purpose of the standard for which a modification is requested. The primary purpose of the Development Standards for the Northwest Plan District is to “foster a transit-supportive, mixed-use urban character with a high-quality pedestrian environment, and an emphasis on good building design”, and the purpose of the Height Bonus for Residential Development is to “encourage residential development, including housing affordable to a range of households; provide incentives for underground parking; and allow taller buildings to screen raised portions of the I-405 freeway”. In this case, the proposal provides a good building design that offers mixed-use development with an active ground floor, high-density residential development above, and a parking solution which has minimal impact on the pedestrian realm, making it consistent with the purpose of the standard requested to be Modified. For the reasons stated above, the proposal is consistent with the intent of the standard.*

The overall solution will better meet the applicable approval criteria and is consistent with the purpose of the standard. *This Modification therefore merits approval.*

2. Standards for all Bicycle Parking (33.266.220.C.3.b).

Standard. A space 2 feet by 6 feet must be provided for each required bicycle parking space, so that a bicycle six feet long can be securely held with its frame supported so that the bicycle cannot be pushed or fall in a manner that will damage the wheels or components.

Proposal. The project includes 120 total long term bicycle parking spaces, which is the amount required by code. The project proposes to use double stack bike racks for 118 of the long-term bike parking spaces, spaced at 1'-6" rather than the 2' spacing required.

Purpose. These standards ensure that required bicycle parking is designed so that bicycles may be securely locked without undue inconvenience and will be reasonably safeguarded from intentional or accidental damage.

Findings:

- A. The resulting development will better meet the applicable design guidelines. The proposed project requires 118 long-term bike parking spaces for residential use, based on a 1 to 1.1 factor of 116 units, and 2 long-term bike parking spaces for retail use. The project is located on a quarter block site, and provides active uses on both frontages. Due to the square footage restrictions of the quarter-block site, the ability to provide a large footprint for bike storage at the ground floor is limited. The project embraces the desire to provide a convenient and safe location for residents to store bicycles. To do so, a bike room has been located such that it can be accessed from both the parking garage and from the interior corridor off the lobby. This makes a clean and efficient circulation pattern for bike usage. The proposed functional and space efficient system better meets the design guidelines because it eases floor plan demands and results in additional opportunities for active uses at the street, such as lobby space and retail tenant spaces. For these reasons,*

the proposal better meets the following applicable approval criteria: *E3. The Sidewalk Level of Buildings* and *D5. Crime Prevention*.

- B. *On Balance, the proposal will be consistent with the purpose of the standard for which a modification is requested.* The primary purpose of the standard is to ensure that required bicycle parking is designed so that bicycles may be securely locked without undue inconvenience and damage. With the designated long-term bike storage room being at grade level, the project proposes to take advantage of the increased podium height by utilizing a double stacking bicycle rack. The proposed 'double-decker' racks are among those within the Handbook of Approved Bicycle Racks (<https://www.portlandoregon.gov/transportation/article/481836#double>), and has been verified as an approved rack by Sarah Figliozzi at BikeParking@PortlandOregon.gov. The proposed bike rack system is engineered to stack bikes vertically to allow the handle bars to overlap. This allows the proposed racks, within an 18" space, to provide the same level of service that would be provided by a standard 24" on center spacing. The staggered clearance between adjacent bikes and allowance for sliding hangers ease the hanging and locking of a bike. A 5' minimum aisle is still provided behind each bicycle rack. The rack system will be located within a bike storage room within the parking garage. *For these reasons, the bicycle parking system is safe and secure, located in a convenient area, and designed to avoid any intentional or accidental damage to bicycles; as such, the proposal is consistent with the purpose statement of the bicycle parking standards.*

The overall solution will better meet the applicable approval criteria and is consistent with the purpose of the standard. *This Modification therefore merits approval.*

3. Loading Standards, Forward Motion (33.266.310.F.1)

Standard. Generally. Outside the Central City Plan District, loading facilities must be designed so that vehicles enter and exit the site in a forward motion.

Proposal. The project proposes that loading access for the two Standard B spaces be forward ingress/egress in one direction, rather than both directions.

Purpose. A minimum number of loading spaces are required to ensure adequate areas for loading for larger uses and developments. These regulations ensure that the appearance of loading areas will be consistent with that of parking areas. The regulations ensure that access to and from loading facilities will not have a negative effect on the traffic safety or other transportation functions of the abutting right-of-way.

Findings:

- A. *The resulting development will better meet the applicable design guidelines.* The proposed loading zone consists of two (2) Standard B Loading Spaces, placed end-to-end within the at-grade drive aisle of the parking garage.

The entrance to the parking garage will be provided via a 20-foot wide driveway accessed from NW Pettygrove, approximately 83' - 5" feet west of the extended curb line on NW 19th Ave. The location the driveway on Pettygrove (also a local service traffic street) is as far away from the NW 19th Avenue intersection as practicable. Speeds are also low on Pettygrove given the stop-control at 19th Avenue.

Loading for the two Standard B spaces proposes back-in or back-out maneuvers, as forward motion is impractical given the site constraints of this one-quarter block development. Auto-turn exhibits could be created showing simulated back-in and back-out maneuvers of a Standard B truck (single unit, 2-axle) at the driveway.

If the proposal were to provide forward ingress and egress for the loading spaces, more of the ground floor would be utilized to accommodate this requirement, allowing less opportunity for active ground floor uses. For these reasons, the proposal better meets the following applicable approval criteria: *E3. The Sidewalk Level of Buildings* and *D5. Crime Prevention*.

- B. *On Balance, the proposal will be consistent with the purpose of the standard for which a modification is requested.* The purpose of the standard is to ensure that access to and from loading facilities will not have a negative effect on the traffic safety or other transportation functions of the abutting right-of-way.

The entrance will include a high-speed (100"/second) spiral door located 2 feet from the property line and approximately 14 feet from the finished curb line. The property is outside the Central City Plan District, and NW Pettygrove Street is classified as "Local Service Traffic Street". And, although it does not apply to the review because the applicable regulations are those in effect at the time that this Land Use application was submitted, the current zoning code has been since revised to add "*Standard B loading spaces that are accessed from a Local Service Traffic Street are exempt from this requirement.*" (33.266.310.F.1).

Given that Pettygrove is classified as "Local Service Traffic Street", the proposal of Loading access to utilize back-in or back-out maneuvers will not impede expected traffic flow within an urban context.

Finally, PBOT has stated they have no objection to the this requested modification (Exhibit E.2). *For the reasons stated above, the proposal is consistent with the intent of the standard.*

The overall solution is consistent with the purpose of the parking development and loading standards. *This Modification therefore merits approval.*

4. Parking Area Layouts, Access to Parking Spaces (33.266.130.F.1.a)

Standard. 1. Access to parking spaces. a. All parking areas, except stacked parking areas, must be designed so that a vehicle may enter or exit without having to move another vehicle.

Proposal. The project proposes to locate on-site loading in the driveway aisle, temporarily blocking 24 of the mechanical entry parking stalls as well as the elevator control room door.

5. Parking Area Layouts, Parking Space and Aisle Dimensions (33.266.130.F.2)

Standard. Parking spaces and aisles must meet the minimum dimensions contained in Table 266-4. The Minimum Parking Space and Aisle Dimensions for 90-degree angled parking is 8'-6" wide by 16' long.

Proposal. The project proposes to decrease the width of the 34 mechanical parking spaces to 8'-0" wide, rather than the 8'-6" spacing required.

Purpose Statement for Modifications 4 and 5. The development standards promote vehicle areas which are safe and attractive for motorists and pedestrians. Vehicle area locations are restricted in some zones to promote the desired character of those zones. Together with the transit street building setback standards in the base zone chapters, the vehicle area restrictions for sites on transit streets and in Pedestrian Districts:

- Provide a pedestrian access that is protected from auto traffic; and
- Create an environment that is inviting to pedestrians and transit users.

The parking area layout standards are intended to promote safe circulation within the parking area, provide for the effective management of stormwater runoff from vehicle areas, and provide for convenient entry and exit of vehicles.

Findings for Modification 4 and 5:

- A. *The resulting development will better meet the applicable design guidelines. Minimizing the area of ground floor space that is taken up with inactive uses, such as loading and vehicle parking spaces, allows greater opportunity for critical ground floor frontages to be utilized for active uses. For these reasons, the proposal better meets the following applicable approval criteria: D7. Blending into the Neighborhood, E3. The Sidewalk Level of Buildings and D8. Interest, Quality, and Composition.*
- B. *On balance, the proposal will be consistent with the purpose of the standard for which a modification is requested.*

For Modification 4:

The proposed loading zone consists of two (2) Standard B Loading Spaces, placed end-to-end within the at-grade drive aisle of the parking garage.

The entrance to the parking garage will be provided via a 20-foot wide driveway accessed from NW Pettygrove, approximately 83' - 5" feet west of the extended curb line on NW 19th Ave. The location the driveway on Pettygrove (also a local service traffic street) is as far away from the NW 19th Avenue intersection as practicable. Speeds are also low on Pettygrove given the stop-control at 19th Avenue.

The entrance will include a high-speed (100"/second) spiral door located 2 feet from the property line and approximately 14 feet from the finished curb line. The property is outside the Central City plan district, and NW Pettygrove Street is classified as "Local Service Traffic Street", which in current zoning code allows for the two Standard B spaces to be exempt from forward motion. Loading for the two Standard B spaces proposes back-in or back-out maneuvers, as forward motion is impractical given the site constraints of this one-quarter block development. AutoTurn exhibits could be created showing simulated back-in and back-out maneuvers of a Standard B truck (single unit, 2-axle) at the driveway.

Given that Pettygrove is classified as "Local Service Traffic Street", the proposal of Loading access to utilize back-in or back-out maneuvers will not impede expected traffic flow within an urban context.

PBOT has stated they have no objection to the requested modification to the location of the loading space (Exhibit E.2).

For Modification 5:

Northwest Plan district has no minimum parking requirements; however, this project proposes 34 parking stalls for private residential use.

The project proposes to maximize parking stalls using mechanized parking. The mechanized parking vendor is Harding Steel and the mechanized parking product is called CarMatrix (refer to C-46). The proposed CarMatrix layout provides 3 levels of parking stalls - one being a below grade pit. CarMatrix operates on a platform-based grid system. The grade level platforms move laterally while the upper and lower platforms move vertically to allow the needed vehicle to be accessed at grade level. For each grouping of platforms (which for this project is a total of two platforms) there will always be a vacant space at

grade for the upper and lower vehicles to be accessed. CarMatrix works such that the car stalls are connected between column bays. The layout for the car stalls are therefore based on the total length of the car matrix. The project proposes a total length of the CarMatrix that provides for 8'-6" per car stall along the West side of the garage, and 8'-2" along the East side of the garage. The entry of a vehicle into the CarMatrix is restricted at any one time to one location at the ground floor level. Each ground floor stall provides curbs that guide the tires within the entry bay. Therefore, drivers are not required to park in such a way as to determine their own spacing between cars, as the CarMatrix will restrict parking outside of any bay with curbs that separate the cars. This feature prevents damage to other vehicles that may occur in standard parking garages.

The CarMatrix provides comprehensive safety features using restrictive gates and keypad controls. The project finds that the proposed parking system meets intent of the zoning code in providing ease of use for parking a vehicle, and protecting the safety of both users and property.

For the reasons stated above, these proposals are consistent with the intent of the standard.

The overall solutions will better meet the applicable approval criteria and are consistent with the purpose of the standard. *This Modification therefore merits approval.*

The following additional Modification has been identified by staff:

6. Ground Floor Windows (33.140.230).

Standard.

- B. Required amounts of window area. In the EX zone, all exterior walls on the ground level which are 20 feet or closer to a street lot line, sidewalk, plaza, or other public open space or right-of-way must have windows. The windows must be at least 50 percent of the length and 25 percent of the ground level wall area. Ground level wall areas include all exterior wall areas up to 9 feet above the finished grade. The requirement does not apply to the walls of residential units, and does not apply to the walls of parking structures when set back at least 5 feet and landscaped to at least the L2 standard.
- C. Qualifying window features. Required window areas must be either windows that allow views into working areas or lobbies, pedestrian entrances, or display windows set into the wall. Display cases attached to the outside wall do not qualify. The bottom of the windows must be no more than 4 feet above the adjacent exterior grade.

Proposal. Staff identified that the proposal provides 40% rather than the required 50% minimum length required of qualifying windows.

Purpose. In the EX zone, blank walls on the ground level of buildings are limited in order to:

- Provide a pleasant, rich, and diverse pedestrian experience by connecting activities occurring within a structure to adjacent sidewalk areas, or allowing public art at the ground level;
- Encourage continuity of retail and service uses;
- Encourage surveillance opportunities by restricting fortress-like facades at street level; and
- Avoid a monotonous pedestrian environment.

Findings:

- A. *The resulting development will better meet the applicable design guidelines. This has not yet been illustrated.*

B. *On balance, the proposal will be consistent with the purpose of the standard for which a modification is requested. This has not yet been illustrated.*

The overall solution has not yet shown that it will better meet the applicable approval criteria. ***This Modification does not yet merit approval.***

DEVELOPMENT STANDARDS

Unless specifically required in the approval criteria listed above, this proposal does not have to meet the development standards in order to be approved during this review process. The plans submitted for a building or zoning permit must demonstrate that all development standards of Title 33 can be met, or have received an Adjustment or Modification via a land use review prior to the approval of a building or zoning permit.

CONCLUSIONS

The design review process exists to promote the conservation, enhancement, and continued vitality of areas of the City with special scenic, architectural, or cultural value. This proposal will provide an active ground level on NW 19th with an architectural expression above that is responsive to the surrounding context.

However, Design Review Staff does **not yet recommend approval** due to concerns with the proposal not meeting the applicable standards and design guidelines along NW Pettygrove. Two items remain unresolved at the time of this Staff report:

1. NW Pettygrove ground floor elevation:

- a. D4. Parking Areas and Garages. Large storefront windows to the west of the garage door on NW Pettygrove Street open directly into the mechanized parking. ***Views into a parking structure are not considered an attractive or complementary resolution.***
- b. Ground Floor Windows (33.140.230.C). Proposal provides 40% rather than the required 50% minimum length required of qualifying windows. ***It has not yet been illustrated how this Modification merits approval.***

2. **Development proposed in the Right-of-Way.** The building step back at the west end of the elevation is not large enough to allow for foundations for the proposed cable-mounted vines intended to be in front of the mechanical parking windows (Exhibits C.6, C.34 and C.36). ***PBOT have advised that this is not allowed.***

The proposal **does not yet meet** the applicable design guidelines and modification criteria and therefore does not yet warrant approval.

TENTATIVE STAFF RECOMMENDATION

(May be revised upon receipt of new information at any time to the Design Commission decision)

Denial. If the applicant can resolve the two items identified above, Staff can then recommend approval the following:

A new seven-story mixed-use building including 107 residential units, ground level retail (approximately 1,150 SF), 34 mechanized parking spaces, one ADA parking space, two (2) Type B loading spaces and 120 long-term bike parking spaces.

The following five (5) **Modification Reviews**:

1. **Height Bonus for Residential Development** (33.562.230.D). Maximum building height allowed is 75'. The project proposes to increase maximum height by 3'-8", to 78'-8"
2. **Standards for all Bicycle Parking** (33.266.220.C.3.b). The project proposes to use double stack bike racks for 118 of the long-term bike parking spaces, spaced at 1'-6" rather than the 2' spacing required.
3. **Loading Standards, Forward Motion** (33.266.310.F.1). The project proposes that loading access for the two Standard B spaces be forward ingress/egress in one direction, rather than both directions.
4. **Parking Area Layouts (33.266.130.F.1)**. The project proposes to locate on-site loading in the driveway aisle, temporarily blocking 24 of the mechanical entry parking stalls as well as the elevator control room door.
5. **Size of Auto Parking Spaces** (33.266.130.F.2) The project proposes to decrease the width of the 34 mechanical parking spaces to 8'-0" wide, rather than the 8'-6" spacing required.

Per Exhibits C.1-C.56, subject to the following conditions:

- A. As part of the building permit application submittal, the following development-related conditions (B through C) must be noted on each of the 4 required site plans or included as a sheet in the numbered set of plans. The sheet on which this information appears must be labeled "ZONING COMPLIANCE PAGE - Case File LU 17-111569 DZM ". All requirements must be graphically represented on the site plan, landscape, or other required plan and must be labeled "REQUIRED."
- B. At the time of building permit submittal, a signed Certificate of Compliance form (<https://www.portlandoregon.gov/bds/article/623658>) must be submitted to ensure the permit plans comply with the Design/Historic Resource Review decision and approved exhibits.
- C. No field changes allowed.

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Procedural Information. The application for this land use review was submitted on January 26, 2017, and the applicant requested it to be determined to be complete on **July 12, 2017** (Exhibit A.4). On July 14, 2017, the applicant requested the proposal to be put on hold to allow time for additional information to be provided (Exhibit A.5). On December 21, 2018, the applicant provided additional information, and A Request for Response was mailed on **December 22, 2017** to public agencies and recognized organizations within 1,000 feet of the site. A Notice of a Public Hearing on a Proposal in Your Neighborhood was mailed on **January 24, 2018**. The proposal is scheduled for the **February 15, 2018** Portland Design Commission hearing.

Zoning Code Section 33.700.080 states that Land Use Review applications are reviewed under the regulations in effect at the time the application was submitted, provided that the application is complete at the time of submittal, or complete within 180 days. Therefore, this application was reviewed against the Zoning Code in effect on January 26, 2017.

ORS 227.178 states the City must issue a final decision on Land Use Review applications within 120-days of the application being deemed complete. The 120-day review period may be waived or extended at the request of the applicant. In this case, the applicant waived the 120-day review period, as stated with Exhibit (Exhibit A.2). Unless further extended by the applicant, **the 120 days will expire on: July 12, 2018**

Some of the information contained in this report was provided by the applicant. As required by Section 33.800.060 of the Portland Zoning Code, the burden of proof is on the applicant to show that the approval criteria are met. The Bureau of Development Services has independently reviewed the information submitted by the applicant and has included this information only where the Bureau of Development Services has determined the information satisfactorily demonstrates compliance with the applicable approval criteria. This report is the recommendation of the Bureau of Development Services with input from other City and public agencies.

This report is not a decision. The review body for this proposal is the Design Commission who will make the decision on this case. This report is a recommendation to the Design Commission by the Bureau of Development Services. The review body may adopt, modify, or reject this recommendation. The Design Commission will make a decision about this proposal at the hearing or will grant a continuance. Your comments to the Design Commission can be mailed, c/o the Design Commission, 1900 SW Fourth Ave., Suite 5000, Portland, OR 97201 or faxed to 503-823-5630.

You will receive mailed notice of the decision if you write a letter received before the hearing or testify at the hearing, or if you are the property owner or applicant. You may review the file on this case by appointment at our office at 1900 SW Fourth Ave., Suite 5000, Portland, OR 97201. Please call the file review line at 503-823-7617 to schedule an appointment.

Appeal of the decision. The decision of the Design Commission may be appealed to City Council, who will hold a public hearing. If you or anyone else appeals the decision of the Design Commission, City Council will hold an evidentiary hearing, one in which new evidence can be submitted to them. Upon submission of their application, the applicant for this land use review chose to waive the 120-day time frame in which the City must render a decision. This additional time allows for any appeal of this proposal to be held as an evidentiary hearing.

Who can appeal: You may appeal the decision only if you write a letter which is received before the close of the record for the hearing, if you testify at the hearing, or if you are the property owner/applicant. **Appeals must be filed within 14 days of the decision. An appeal fee of \$5,000.00 will be charged.**

Additional information on how to file and the deadline for filing an appeal will be included with the decision. Assistance in filing the appeal and information on fee waivers are available from the Bureau of Development Services in the Development Services Center, 1900 SW Fourth Ave., First Floor. Neighborhood associations recognized by the Office of Neighborhood Involvement may qualify for a waiver of the appeal fee provided that the association has standing to appeal. The appeal must contain the signature of the Chair person or other person authorized by the association, confirming the vote to appeal was done in accordance with the organization's bylaws.

Neighborhood associations, who wish to qualify for a fee waiver, must complete the Type III Appeal Fee Waiver Request for Organizations Form and submit it prior to the appeal deadline. The Type III Appeal Fee Waiver Request for Organizations Form contains instructions on how to apply for a fee waiver, including the required vote to appeal.

Recording the final decision.

If this Land Use Review is approved the final decision will be recorded with the Multnomah County Recorder.

- *Unless appealed*, the final decision will be recorded two weeks after the decision is mailed by the Bureau of Development Services.

The applicant, builder, or a representative does not need to record the final decision with the Multnomah County Recorder.

For further information on your recording documents please call the Bureau of Development Services Land Use Services Division at 503-823-0625.

Expiration of this approval. An approval expires three years from the date the final decision is rendered unless a building permit has been issued, or the approved activity has begun.

Where a site has received approval for multiple developments, and a building permit is not issued for all of the approved development within three years of the date of the final decision, a new land use review will be required before a permit will be issued for the remaining development, subject to the Zoning Code in effect at that time.

Zone Change and Comprehensive Plan Map Amendment approvals do not expire.

Applying for your permits. A building permit, occupancy permit, or development permit must be obtained before carrying out this project. At the time they apply for a permit, permittees must demonstrate compliance with:

- All conditions imposed here.
- All applicable development standards, unless specifically exempted as part of this land use review.
- All requirements of the building code.
- All provisions of the Municipal Code of the City of Portland, and all other applicable ordinances, provisions and regulations of the city.

The Bureau of Development Services is committed to providing equal access to information and hearings. Please notify us no less than five business days prior to the event if you need special accommodations. Call 503-823-7300 (TTY 503-823-6868).

Grace Jeffreys

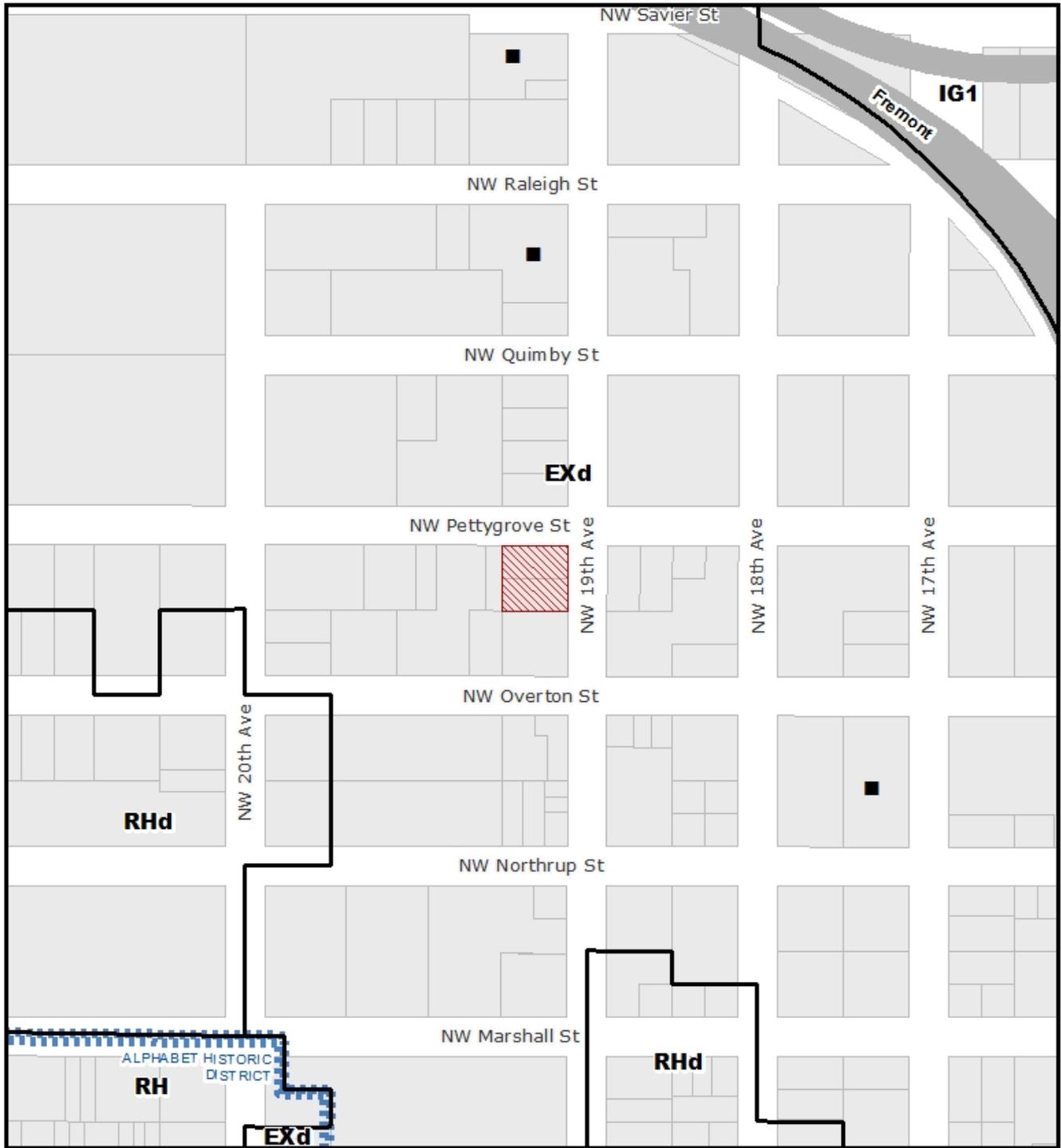
February 8, 2018

EXHIBITS – NOT ATTACHED UNLESS INDICATED

- A. Applicant's Statement
1. Initial submittal, 1/26/17
 2. Request for an Evidentiary Hearing and Waiver or Right of a Decision within 120 Days, 2/3/17
 3. Address correction from Applicant, 2/27/17
 4. Applicant letter requesting application to be deemed "Complete", 7/12/17
 5. Applicant letter requesting application be put on "Hold", 7/14/17
 6. Revised draft proposal, 10/6/17
 7. Check for 4 design Modifications, 12/13/17
 8. Stormwater Report, 12/13/17
 9. Revised narratives, 12/15/17
 10. Revised drawings, 12/15/17
 11. Driveway Design Exception Decision Form, 1/8/18

12. Image of proposed driveway door
13. Package for Life Safety meeting,
14. Revised appendix, including standard compliance diagrams, 2/7/18
- B. Zoning Map (attached)
- C. Plan & Drawings
 1. Cover Page
 2. NOT USED
 3. Index
 4. NOT USED
 5. Site Plan (attached)
 6. Ground Floor Plan
 7. Level 2 Floor Plan
 8. Levels 3-7 Floor Plan
 9. Roof Plan
 10. North Elevation
 - 10b. North Elevation, line drawing (attached)
 11. East Elevation
 - 11b. East Elevation, line drawing (attached)
 12. South Elevation
 - 12b. South Elevation, line drawing (attached)
 13. West Elevation
 - 13b. West Elevation, line drawing (attached)
 14. Building Section A
 15. Building Section B
 16. Building Section C
 17. Building Section D
 18. Wall Section 1
 19. Wall Section 2
 20. Wall Section 3
 21. Wall Section 4
 22. Wall Section 5
 23. Wall Section 6
 24. Materials/Colors
 25. Materials/Colors
 26. NOT USED
 27. Metal Panel 1
 28. Metal Panel 2
 29. Metal Panel 3
 30. Landscape Design
 31. Landscape Design
 32. Landscape Design
 33. Landscape Design
 34. Landscape Details
 35. Landscape Details
 36. Ground Floor Lighting
 37. Terrace Lighting
 38. NOT USED
 39. Ground Floor Planted Area
 40. Civil Design
 41. Civil Design
 42. Product information - residential windows
 43. Product information - aluminum and glass storefront
 44. Product information - glass guardrails
 45. Product information - metal panel
 46. Product information - vertical car stackers

47. Product information - bike parking racks
 48. Product information - off -site short term bike parking racks
 49. Product information - high speed coiling garage door
 50. Product information - roof op equipment
 51. Product information - cable-mounted vine system
 52. Product information - exterior lighting
 53. Product information - exterior lighting
 54. Product information - exterior lighting
 55. Product information - exterior lighting
 56. Product information - exterior lighting
- D. Notification information:
1. Request for response
 2. Posting letter sent to applicant
 3. Notice to be posted
 4. Applicant's statement certifying posting
 5. Mailed notice
 6. Mailing list
- E. Agency Responses:
1. Bureau of Environmental Services
 2. Bureau of Transportation Engineering and Development Review
 3. Fire Bureau
 4. Bureau of Parks, Forestry Division
 5. Life Safety Review Section of BDS
- F. Letters
1. Katie Pitello January 26, 2018, would like to see more parking spaces provided.
- G. Other
1. Original LUR Application
 2. Pre-application Conference summary, conference date 1/26/17
 3. Site photos
 4. Incomplete letter, 2/14/17
 5. Request for Completeness review with PBOT & BES responses, 2/17/17
 6. Staff response to email from Lee Fehrenbacher regarding Bonuses, 4/10/17
 7. Notice of potential voiding of LU application, 6/19/17



ZONING



THIS SITE LIES WITHIN THE:
NORTHWEST PLAN DISTRICT



Site



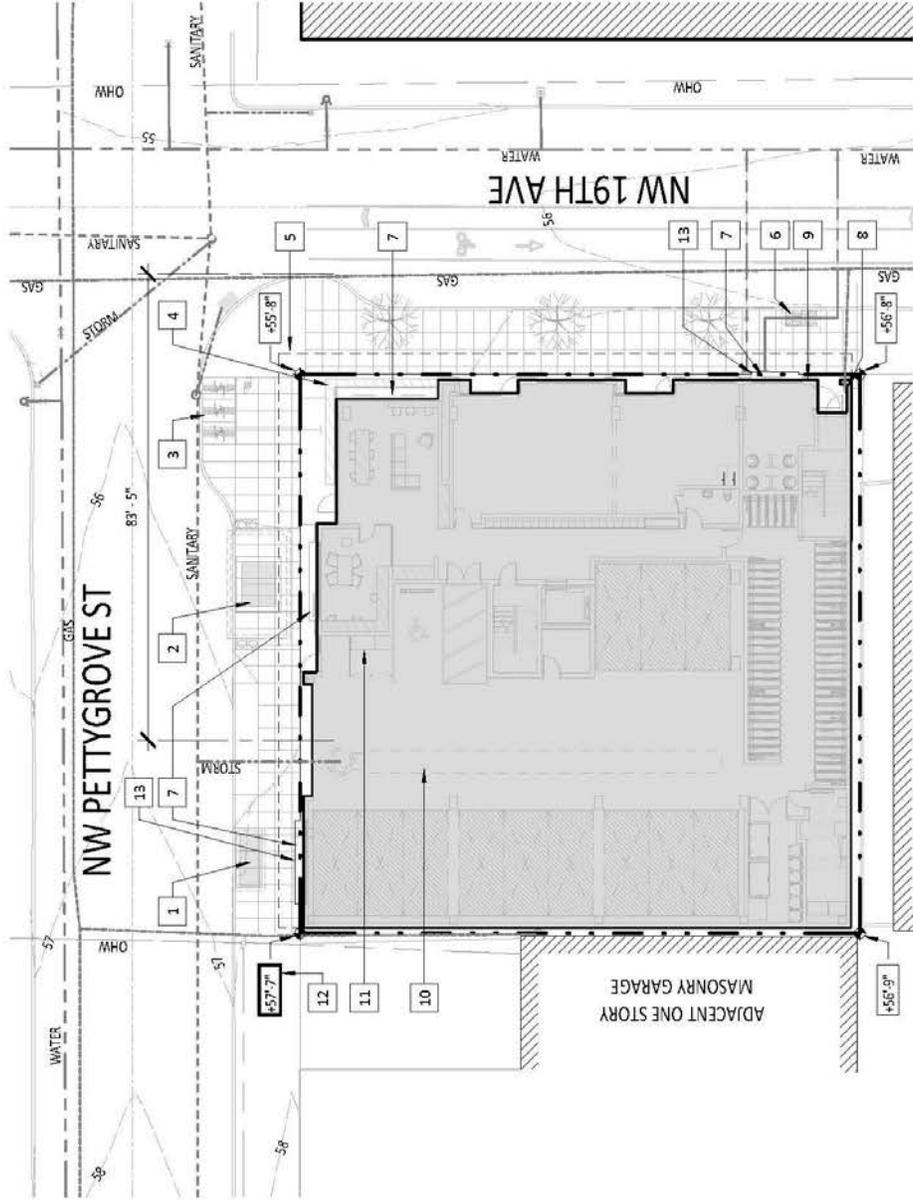
Historic Landmark



Bridge

File No.	LU 17-111569 DZM
1/4 Section	2928
Scale	1 inch = 200 feet
State ID	1N1E33AB 9800
Exhibit	B Dec 20, 2017

SITE PLAN



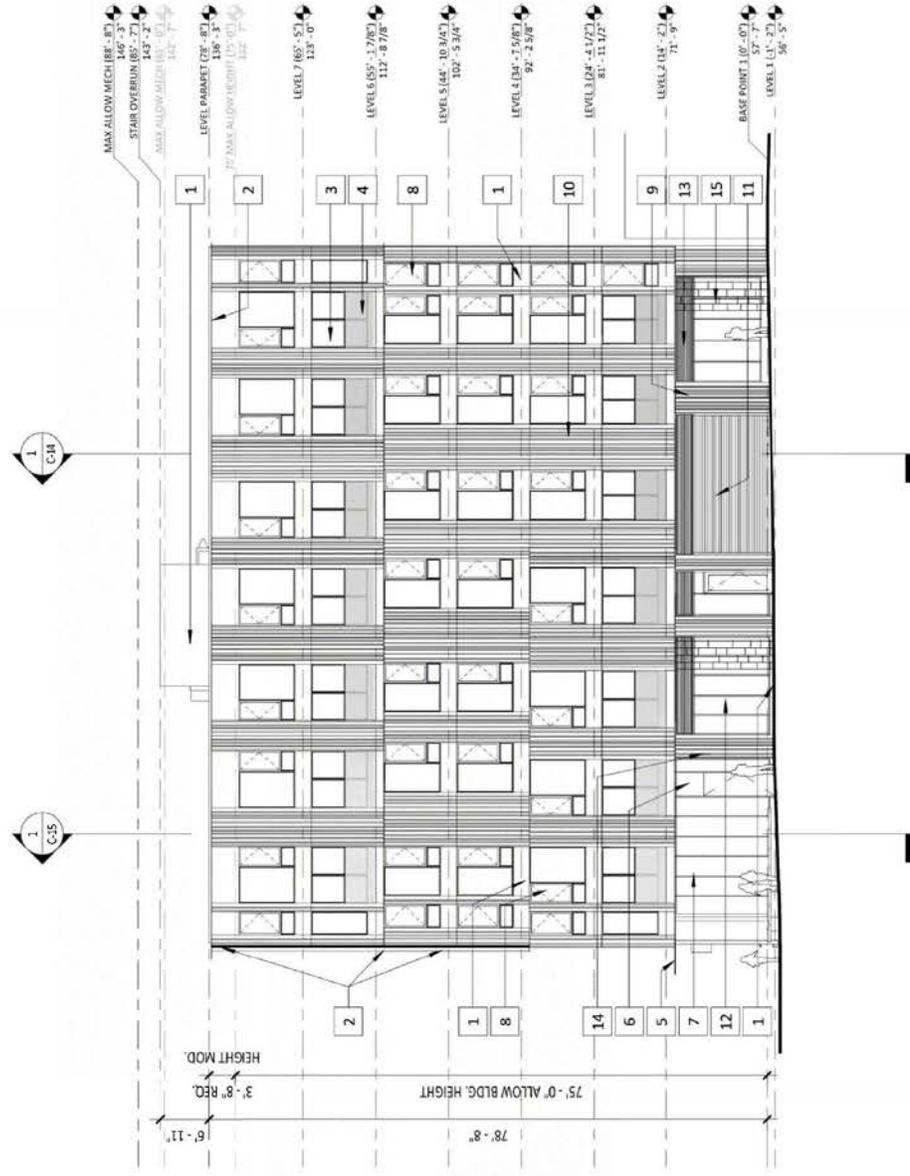
KEY ANNOTATION LEGEND

1	TRANSFORMER VAULT
2	ELECTRICAL VAULT AND VENTS
3	SHORT TERM BIKE PARKING
	PROJECT TO PAY INTO CITY BIKE PROGRAM
4	SEATING BENCH
5	LINE OF CANOPY ABOVE
	5 FOOT EXTENDED OVER R.O.W.
6	WATER METER
7	CABLE MOUNTED VINES
8	GAS METER
9	FDC
10	ON SITE STORMWATER DETENTION PIPE
11	ELECTRICAL MAIN SWITCH
12	BASE POINT 1:
	ZONING HEIGHT REFERENCE
13	PLANTING AREA IN FRONTAGE ZONE, REFER TO C39

1 ARCHITECTURAL SITE PLAN
SCALE: 1" = 32'-0"



NORTH ELEVATION



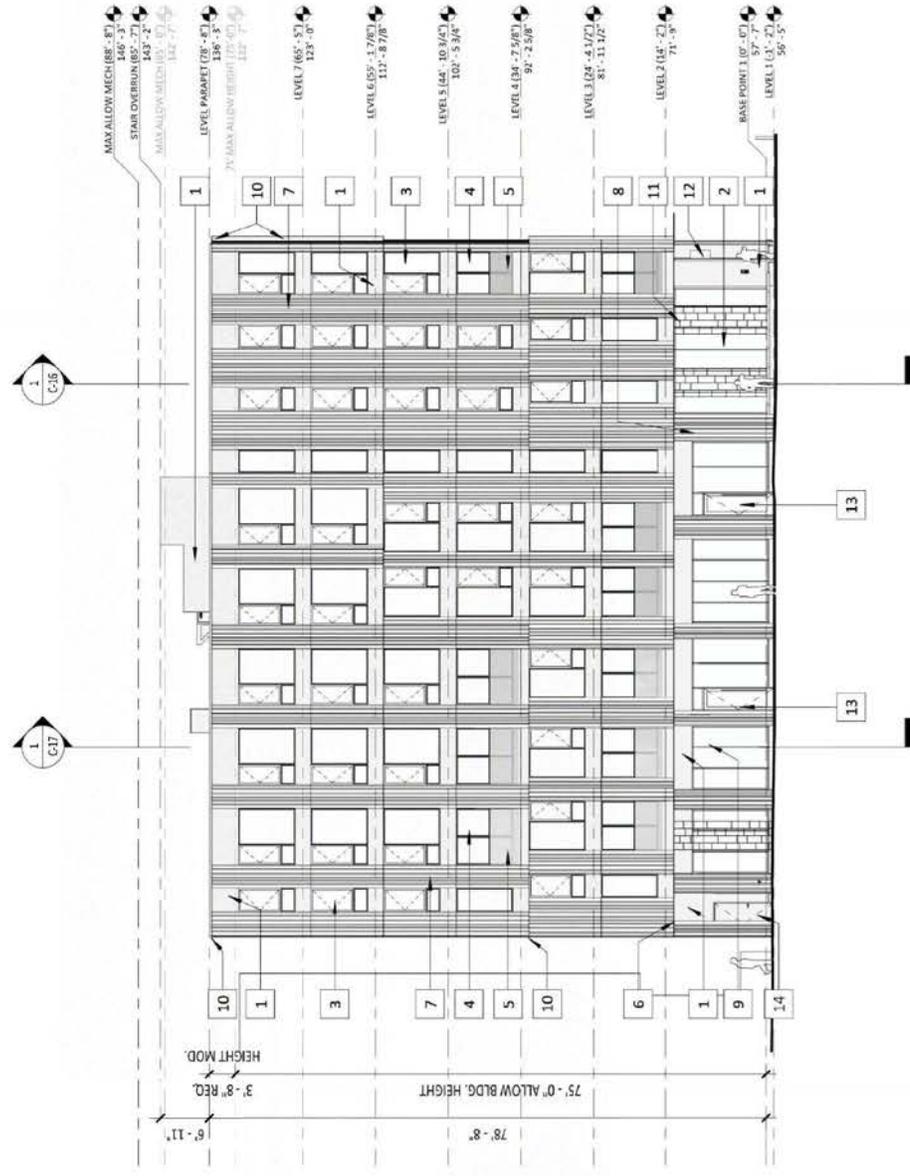
KEY ANNOTATION LEGEND

- 1 METAL PANEL 3 - MATTE CHARCOAL BLACK ON HAT CHANNEL
- 2 BRAKE METAL FLASHING
- 3 SLIDING DOOR
- 4 GLASS GUARDRAIL
- 5 STEEL CANOPY
- 6 WOOD CLAD ENTRY DOOR
- 7 CURTAIN WALL AT LOBBY
- 8 VINYL WINDOW TYP.
- 9 CONCRETE PIERS
- 10 METAL PANEL 1 - SILVER RIBBED ON Z GIRT
- 11 METAL SECTIONAL GARAGE DOOR
- 12 STOREFRONT
- 13 LOUVER
- 14 BUILDING MOUNTED SIGN
- 15 CABLE VINES

1 NORTH ELEVATION
SCALE: 1/8" = 1'-0"



EAST ELEVATION



KEY ANNOTATION LEGEND

- 1 METAL PANEL 3 - MATTE CHARCOAL BLACK ON HAT CHANNEL
- 2 CURTAIN WALL AT LOBBY
- 3 VINYL WINDOW TYP.
- 4 SLIDING DOOR
- 5 GLASS GUARDRAIL
- 6 STEEL CANOPY
- 7 METAL PANEL 1 - SILVER RIBBED ON Z GIRT
- 8 CONCRETE PIERS
- 9 STOREFRONT
- 10 BRAKE METAL FLASHING
- 11 CABLE VINES
- 12 BUILDING MOUNTED SIGN
- 13 WOOD CLAD ENTRY DOOR
- 14 DOOR PAINTED TO MATCH METAL PANEL 3

1 EAST ELEVATION
SCALE: 1/8" = 1'-0"



SOUTH ELEVATION



WEST ELEVATION



- KEY ANNOTATION LEGEND**
- 1 METAL PANEL 3 - MATTE CHARCOAL BLACK ON HAT CHANNEL
 - 2 VINYL WINDOW TYP.
 - 3 SLIDING DOOR
 - 4 GLASS GUARDRAIL
 - 5 METAL PANEL 2 - SILVER RIBBED ON HAT CHANNEL
 - 6 STEEL CANOPY
 - 7 BRAKE METAL FLASHING

1 WEST ELEVATION
SCALE: 1/8" = 1'-0"